



Comhairle Contae Thiobraid Árann
Tipperary County Council



Suir Island Masterplan



KENNETH HENNESSY ARCHITECTS Ltd., Clonmel

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List of Abbreviations

AADT	Annual Average Daily Traffic
IW	Irish Water
C+EDP	Clonmel and Environs Development Plan
OPW	Office of Public Works
SAC	Special Area of Conservation
TCC	Tipperary County Council

Executive Summary

Tipperary County Council together with the Tipperary Sports Partnership appointed Kenneth Hennessy Architects, Martin Peters Associates Consulting Engineers and The Planning Partnership to prepare a Masterplan for Suir Island, Clonmel as a community amenity and recreational resource.

The Design Team took as their starting point the previous surveys, reports and proposals in relation to heritage, archaeology, tourism and environment previously undertaken in relation to the Island and its surroundings.

Additionally, the design team engaged in a thorough consultation process with members of the public and stakeholders to gather their views and ideas for the development of the island in the context of the brief.

The previous studies and the consultation process informed the preparation of the masterplan proposals. The proposed masterplan therefore takes into consideration the complexities of the site (location in the Lower River Suir SAC; rich heritage and history - including a protected structure on site; location within a flood risk zone) and has developed the opportunities within these constraints through a multi-disciplinary design process. This comprehensive, best-practice Masterplan report and drawings promotes the development of Suir Island as a recreational amenity; a robust, sustainable and appropriate form of development for this key public asset and resource.

This primary purpose of this report is to summarise the masterplan process undertaken by the design team and the resulting proposals to inform future proposals for the development of this site as a recreational and community resource.

This report details:

- The opportunities and constraints presented by the site,
- The consultation process,
- The masterplan proposal details,
- The implementation of the masterplan proposals.

This report should be read in conjunction with the full-sized drawings, and images of the proposal plans are given within this document for reference.

The Masterplan proposals documented in this report include the following:

- Increased accessibility to the island via two new pedestrian links via bridges on the north and south side of island, with an adjoining public plaza to the north side,
- Raised walk on the existing flood berm, with adjoining tree-top walk and access points to the remainder of the island,
- Feasibility study for Suir Island House and proposals for the adjoining area as a formal garden,
- Re-instatement of the millrace, and proposals for its potential use,
- Re-instatement of the wildwood clearings, and proposals for a potential range of uses,
- Development of a facilities building including toilets, changing area and landlord / management office,
- Development of a pumping station and mains sewer connection for the island,
- Potential locations for signage, art installations, seating and lighting.

Dated: September 2019

1. Introduction

1.1 Introduction

Suir Island set in the River Suir was once the commercial and industrial heart of the Town, with milling as the primary industry and a trade centre due to the navigation of the River Suir from the port in Waterford to Clonmel. In 2017, Tipperary County Council and Tipperary Sports Partnership agreed to develop a detailed Masterplan with designs for Suir Island in Clonmel as a key community amenity and recreational resource in Clonmel. Suir Island is located in the centre of Clonmel and is within easy walking distance of Clonmel Town Centre and is one of Clonmel's most important attributes, its high-quality natural setting at the foot of the Comeragh Mountains and within the valley of the River Suir.

Since 2012, a number of initiatives through Interreg projects 'Waterways Forward' and Green & Blue Futures' have led to the development of this Masterplan. The development of the River Suir Blueway and canoe slalom course adjacent to Suir Island will form part of the offering and will assist in marketing the town and region as a location for active recreation and living.

The island is surrounded by the River Suir on all sides and is accessible from the town centre via the Old Bridge to the north west of the island. The island, from its mid-section to eastern end is largely undeveloped and overgrown. This section of Suir Island has the potential to be developed as the 'Green Heart' of Clonmel, its central location makes it an ideal location as an amenity hub with both formal and informal amenity and recreational facilities.

A key objective of the development of the Masterplan is to explore the potential opportunities for the development of Suir Island. The long-term aim is to provide the local community with a recreational amenity in keeping with its surroundings which will allow people access to a variety of amenities creating both a social and economic benefit to the area. The development will also aid to reconnect the community of Clonmel with the River Suir and explore the outdoors on Suir Island. It will be a step forward in investment for future generations, improved well-being, addressing social issues in the community along with generating interest in healthy living, sport and fitness along with a connection to the heritage and biodiversity of the island. Suir Island is an important amenity for Clonmel and when developed it will add vitality and vibrancy to the area. In terms of economic development, it will contribute significantly to the attractiveness of the town as a visitor destination and will greatly enhance the quality of life for the inhabitants of the town as a whole.

1.2 Brief and Scope

The project brief is to develop a Masterplan for Suir Island as a community amenity and recreational resource which incorporates designs for suitable options for the development and use of Suir Island.

The proposed Masterplan addresses the brief and provides a document that establishes broad strategic principles that will inform the future development of Suir Island. The Masterplan includes for proposals for:

- the possible type, scale and location of land uses,
- improved movement, connectivity and linkage,
- size and location of public open spaces/ amenities,
- the scale and design of new buildings and proposals to best use existing buildings and features of heritage value,
- an implementation strategy that would enable orderly re-development of the area.

The Masterplan report should be read in conjunction with the Masterplan Drawings:

- SK01 Site Layout Plan,
- SK02 Landscape Plan,
- SK03 Site Sections,
- SK04 Facilities Building and Mains Services,
- SK05 Proposal Views,
- SK06 Schematics.

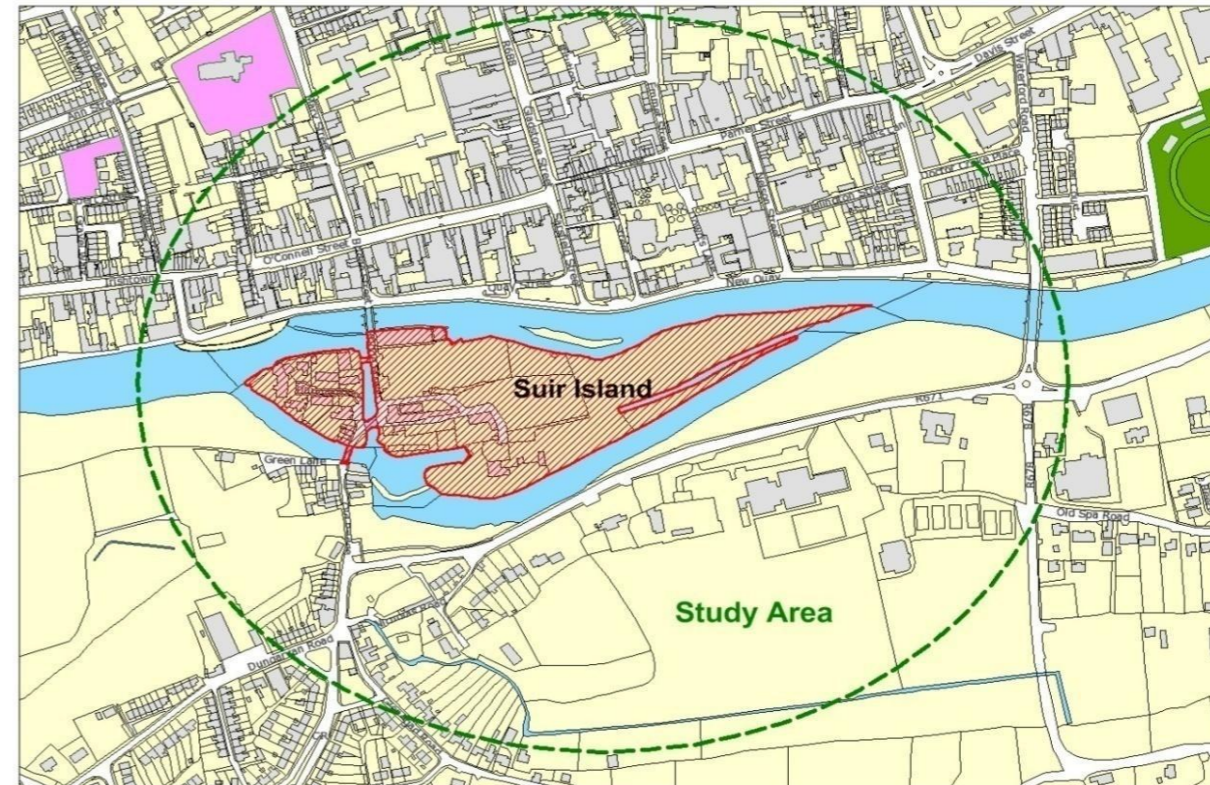


Figure 1: Broad effective study area as per Masterplan Brief

Figure 1 illustrates the broad study area examined in the preparation of the Masterplan and Figure 2 outlines the site area for which detailed proposals are included in the Masterplan.

The Masterplan will provide stakeholders and decision makers with a key tool to guide the future development of Suir Island.

1.2 Methodology

The Masterplan has been prepared as a joint-venture project by a multi-disciplinary project team, with expertise in architecture, planning, urban design, ecology and engineering, working in conjunction with Tipperary County Council and Tipperary Sports Partnership.

The Masterplan has been informed by existing plans, reports and surveys prepared for Suir Island, in particular the "Built Heritage Conservation Action Plan for Suir Island" (Blackwood Associates 2014), the Clonmel Flights of Discovery Report 2017 and the Clonmel and Environs Development Plan 2013, as varied. New surveys were undertaken to address existing data gaps (visual, traffic, topographical, ecological and tree survey) and to inform the proposals.

Consultation with stakeholders and the public was an important part of the preparation of the Masterplan proposals.

The value of Suir Island as a high-amenity site with sensitive ecology and important built heritage was carefully and strategically considered in the preparation of the Masterplan

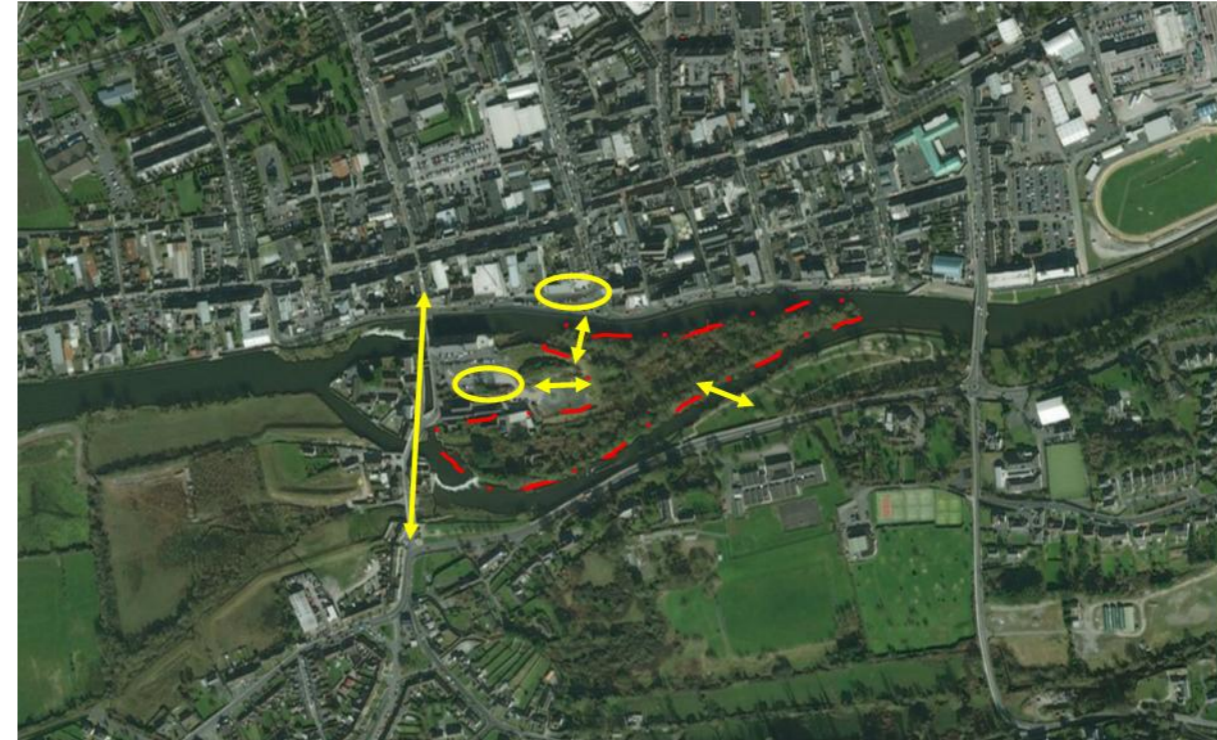


Figure 2: Area extent of proposals in red with adjacent strategy areas in yellow

2. Opportunities & Constraints

2.1 Site Context and Key Characteristics

Suir Island is a naturally occurring island formed within the Suir River channel in Clonmel's town centre. The island is bounded on all sides by the river and there is a canoe slalom course with two river access points along the island's southern boundary. The island is accessible from the town centre via the Old Bridge. The island, from its mid-section to eastern end is largely inaccessible and overgrown. The majority of the island is in the ownership of Tipperary County Council, with some areas in private ownership. Suir Island currently houses an apartment development, surface level car parking, commercial buildings and private residences.

A slalom course with access and portage infrastructure has recently been developed in the river channel on the south side of the island. This is a unique piece of sporting infrastructure and the only of its kind in the southern region outside Dublin. Access to this course is through Suir Island. The slalom course forms part of the Suir Blueway which has seen the development of further access points to the river Suir upstream and downstream of Clonmel and the development of a continuous river walkway linking Clonmel to Carrick on Suir. Suir Island occupies a key location on the Blueway offering access to the river and riverside walkway.

Existing vehicular and pedestrian movement and access to the island is currently via the Old Bridge Road and the Quays. The Old Bridge Road which includes two bridges over the River Suir is variable in width from 4.8 to 5.8m. The road has two-way traffic flow, facilitating traffic travelling south from the town centre and north into the town centre. On and around the island movement is facilitated by the existing laneways and the existing car park (Figure 12). The development of the Slalom Course has seen accessibility to the southern part of the island improved. The majority of the island has a low level of accessibility.

There is an existing public surface level carpark on Suir Island which was recently redeveloped to provide increased car parking provision and a multifunctional space. The redevelopment allows for the future provision of bicycle parking and bus parking, as and when the need arises. There is also a private carpark serving the existing apartments.

The island is located within the 1 in 100-year flood envelope of the River Suir and evacuation of the island is required when flood waters reach the 1:50 year levels. The site is prone to flooding in its eastern and southern parts. The Clonmel Flood Defences have protected parts of the island - the car park and surrounding residences - and the remainder of the island is prone to flooding. The defence works have had a significant impact on the presentation and form of the island with the erection of flood walls and a berm.

An assessment of the rich opportunities for Recreational Amenity, taking account of the island's unique constraints.

Suir Island has a rich industrial heritage and in the 18th Century was densely developed with mills, factories, warehouses and millowner homes. The majority of the historical buildings have been lost, but the Island contains a number of remains and features associated with its industrial past. The island is within a zone of archaeological interest, there is a protected structure on the island (Suir Island House) and the island contains a number of buildings/structures listed on the National Inventory of Archaeological Heritage.

Suir Island acts as a 'green lung' within Clonmel and its trees and vegetation provide an attractive backdrop to the town and give a high amenity value to the island. Suir Island is within the Lower River Suir Special Area of Conservation (Lower River Suir SAC) (Site Code no: 002137) and the island is home to a rich variety of flora and fauna. A Tree Survey and Tree Preservation Plan of the island was undertaken (Figure 8 and Appendix G) and this provided a useful baseline assessment that identified trees to be retained and some to be considered for removal due to their condition and impact on existing structures. An Ecological Survey was also undertaken which provided an understanding of the ecological sensitivities on the island. These surveys and analysis of historical records show existing former clearings on the island that have been overgrown by vegetation.

The island's topography is low lying and significant portions of the site are dominated by mature large-scale trees. The height of these trees lends the generally flat space a degree of visual enclosure and their retention ensures that the introduction of large-scale structures can be successfully integrated without radically altering the site's characteristics. The existing trees and planting provide a number of benefits including enhanced habitat biodiversity, microclimate screening and spatial definition functions as well as aesthetic amenity.

The island is serviceable by the public watermain and part of Island has a surface water drainage collection network. The island is not served by the public sewer. Records from Irish Water show the nearest foul sewers to be located on the Quays and Raheen Road.



Figure 3: Flood defence berm (left); flood defence wall (right)

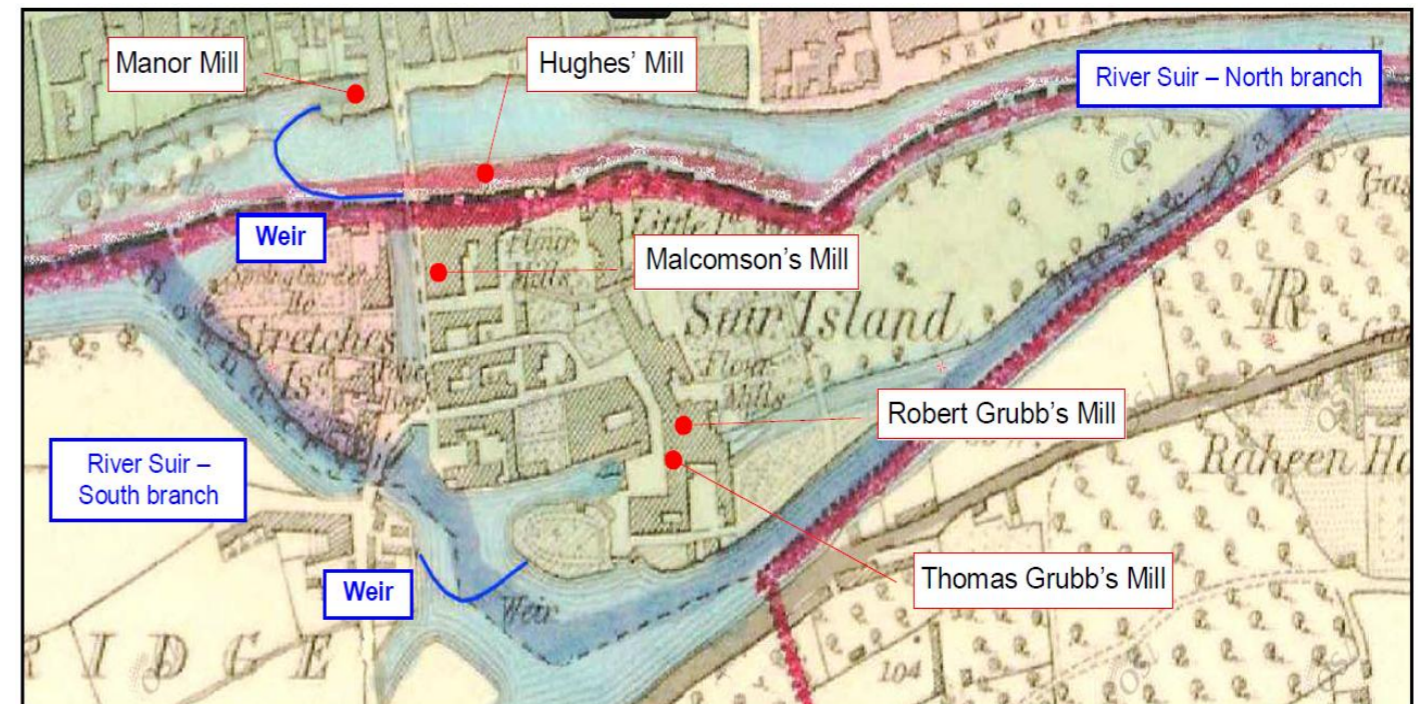


Figure 4: Flour Mills on Suir Island; source: Blackwood Report 2014.



View looking north from on top of the flood berm



Suir Island House



View looking westwards towards Suir Island from North Quay



View looking westwards towards river from south bank of Suir Island

Figure 5: Site Photos (Autumn 2017)

2.3 Opportunities and Considerations

The following Table outlines the key opportunities for the site and considerations associated with the opportunities.

The opportunities include those identified as part of the public and stakeholder consultation.

Opportunities	Considerations / Constraints
<p>Regeneration of a naturally occurring island within Clonmel's town centre into a functioning recreational and amenity resource and regenerate same as a tourism draw.</p> <p>New identity and transformation of character to an activity /recreation/adventure hub anchored on the existing slalom course, key location on the Blueway and natural high-quality amenity parkland. This can be through:</p> <ul style="list-style-type: none"> - Promotion of slalom course as an attraction for events/ training/ centre of excellence. - Promotion of site as a hub for water sports. - Development of natural amenities as parklands, gardens, adventure/sporting activities (archery, tree top walk, zipline), walking trails. <p>Develop facilities and services to support the above i.e. bicycle/ boat storage and rental, showers, lockers, changing and welfare facilities.</p> <p>Improved accessibility to and within the island for all forms of transport with direct pedestrian linkage to Clonmel's town centre and adjoining amenities.</p> <p>Improved physical character to the island and the approaches/gateways to same to develop a new sense of place.</p> <p>Conserve the built heritage remains and integrate same into new uses. Reinstatement former industrial heritage where possible i.e. re-watered mill race.</p> <p>Exploit industrial heritage as tourism draw and for education and interpretation.</p> <p>Active management of ecology and biodiversity of the site, removal of invasive species and creation of new habitat through re-watered mill race.</p> <p>Develop site as base for sports and/or nature clubs/organisations. Provide skills training in nature, sports etc.</p> <p>Enliven the car-park through use of same for markets, events, concerts etc.</p> <p>Use site ecology and heritage for education interpretation</p>	<p>The current access to the island and the roads and infrastructure on the island have limited capacity and development of the island may lead to a need to develop new traffic management arrangements for access to the island, both for normal daily operation and also during events. This would be subject to further traffic assessment and modelling.</p> <p>The existence of a protected structure (Suir Island House) and built heritage remains and the location of the island within a zone of archaeological potential present conservation requirements that need to be met.</p> <p>Part of Suir Island and its environs are located within the Lower River Suir SAC (Site Code 002137) and there is a legislative obligation under the Habitats Directive to ensure conservation objectives relating to the SAC are met. Figure 21 provides a visual indicator of the island and identifies where the highest ecological sensitivities are likely to be found. Appendix J details the potential for Annex i habitats and Annex ii species to occur in the vicinity of Suir Island, as well as a summary of Protected Species and Habitats located relative to Suir Island.</p> <p>The majority of the island is at a high risk of flooding (see Figure 22). This has implications for use, development design etc.</p> <p>Currently the island is not serviced in terms of foul drainage and there is an opportunity to service the entire island with foul drainage. Portable toilet facilities could be used as a short-term solution, especially during events, until a permanent service can be provided. Irish Water was consulted on this matter and has confirmed that the wastewater network in Clonmel has capacity to facilitate the development of Suir Island and provide for future connections to the wastewater and drinking water networks.</p>

Allow for local artists, craftsmen and apprentices to get involved in the design and construction of art and sculpture installations on the island.

Land being under Council ownership presents opportunity for cohesive implementation.

Table 1: Opportunities and Constraints

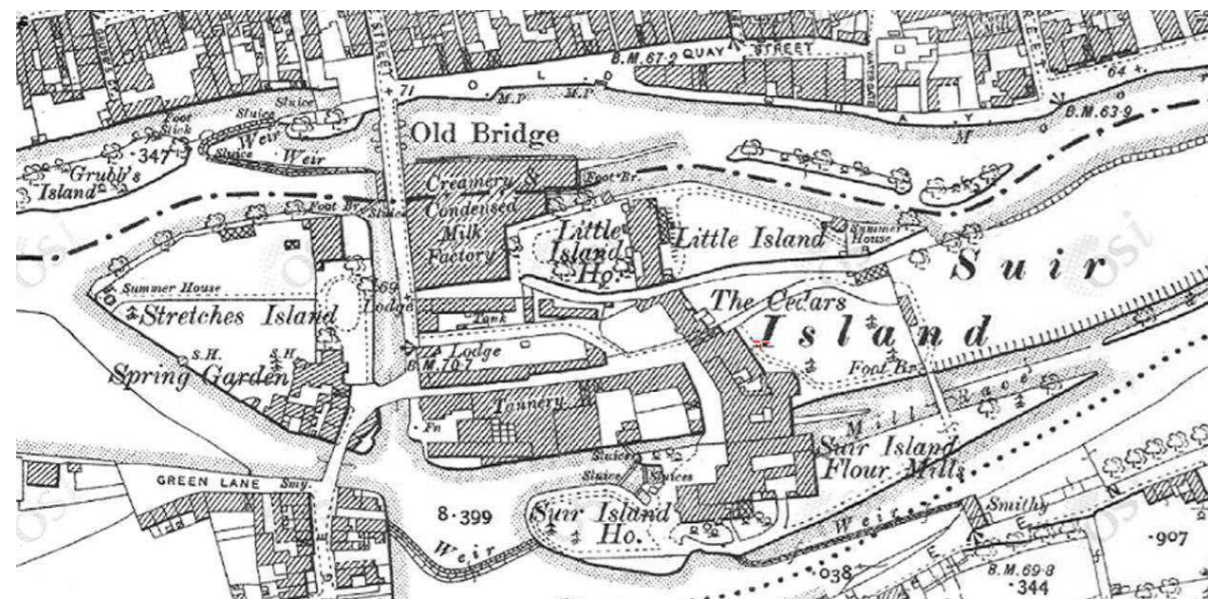


Figure 6: OS 1874 Map; source Blackwood Report 2014

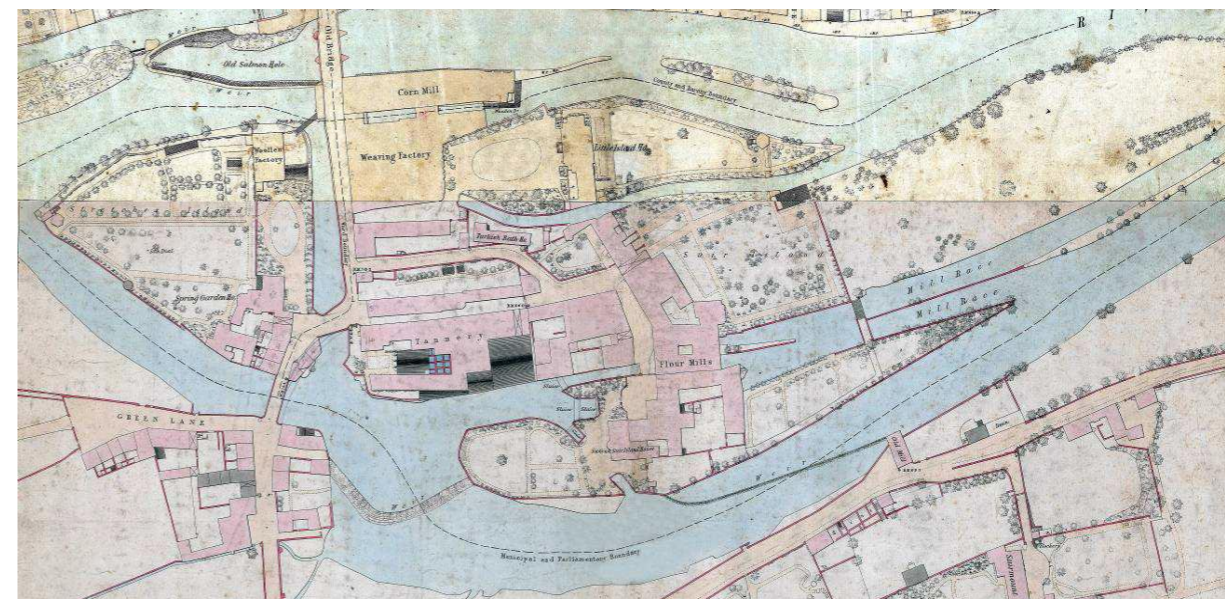


Figure 7: OS 1904 Map; source Blackwood Report 2014

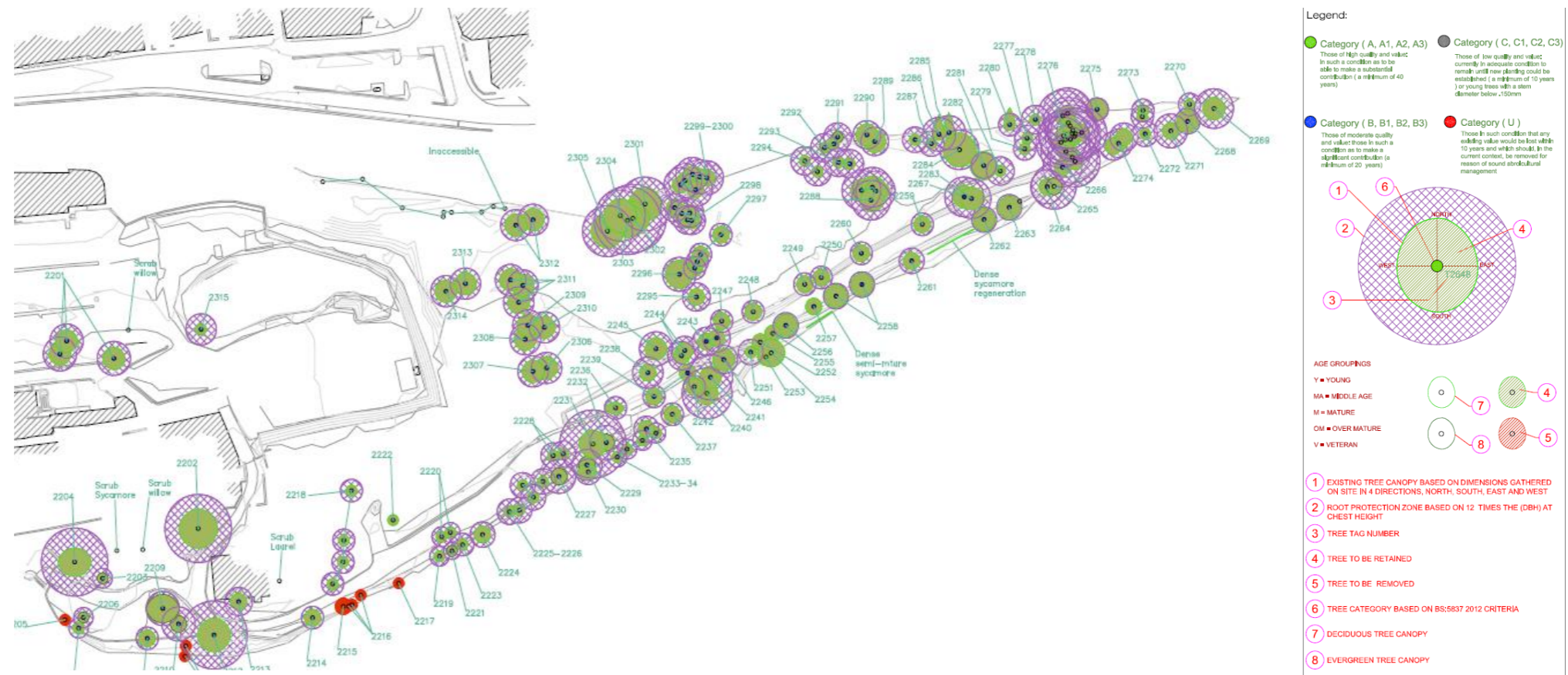


Figure 8: Tree Survey Plan, See Appendix G for detail



Figure 9: View from north Quay looking eastwards to Suir Island



Figure 10: Island trees; photo taken Sept 2017

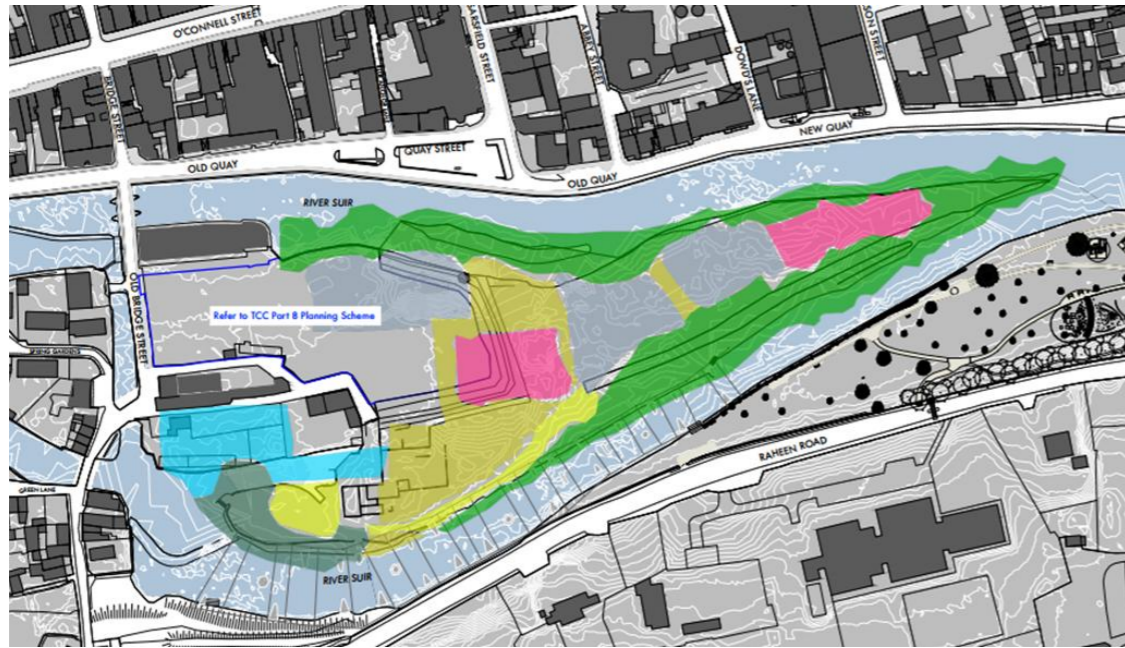


Figure 11: Habitat Boundaries and other Ecological features on Suir Island; source: Feehan, J. and Sheridan, H (2009) River Suir Heritage Survey Volume II Natural Heritage

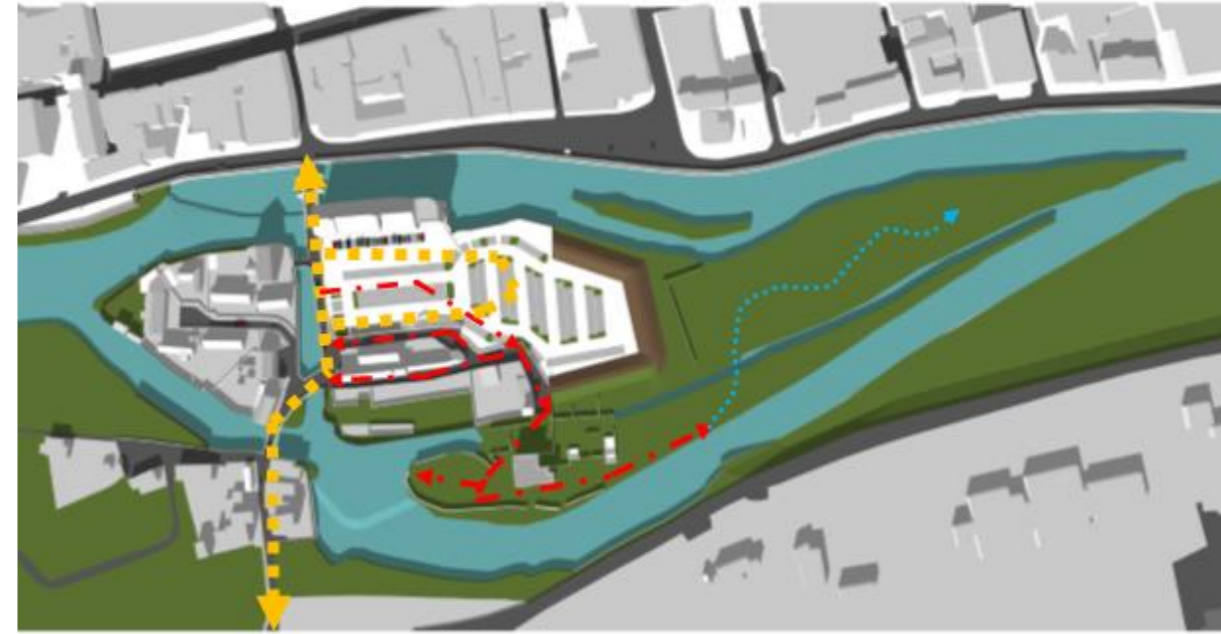
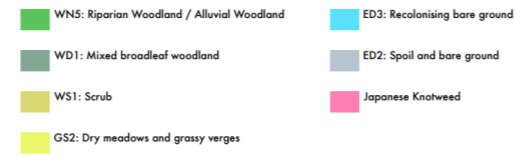


Figure 12: Existing access to the island, with current river access points

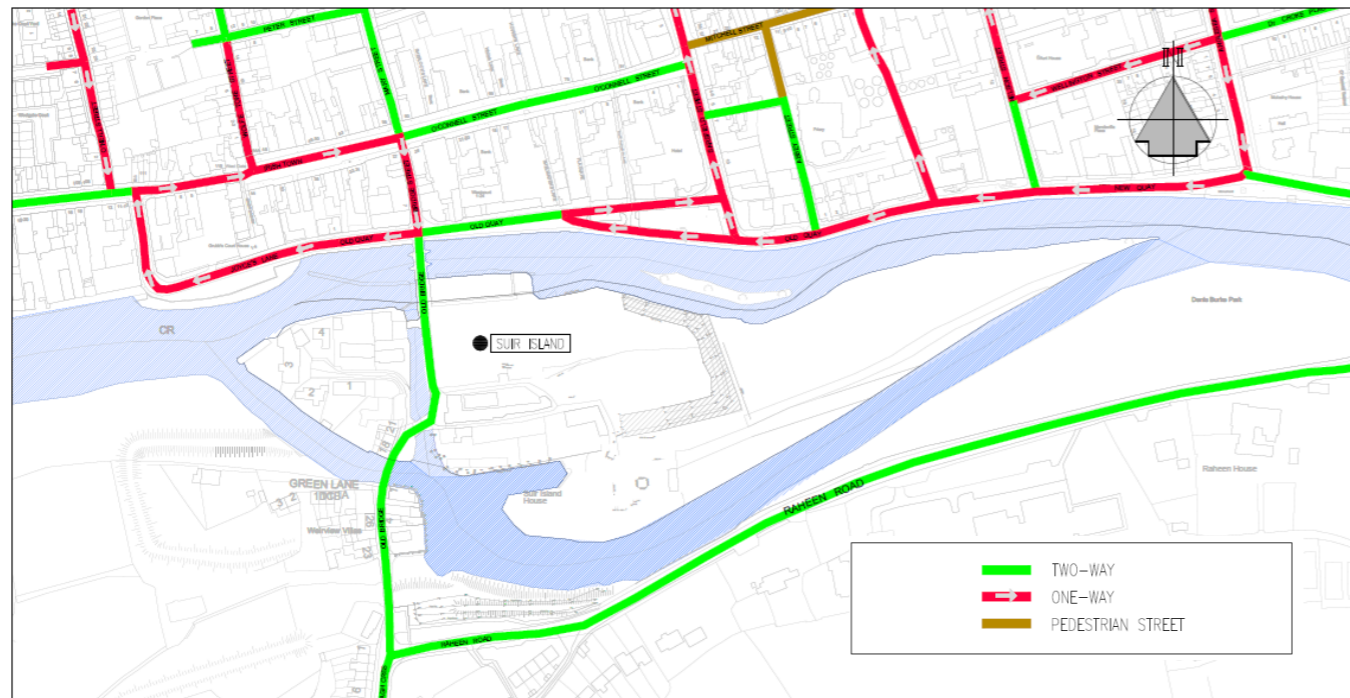
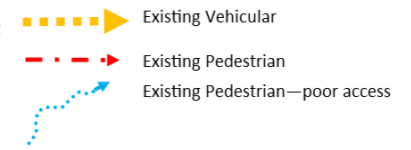


Figure 13: Existing Road Layout

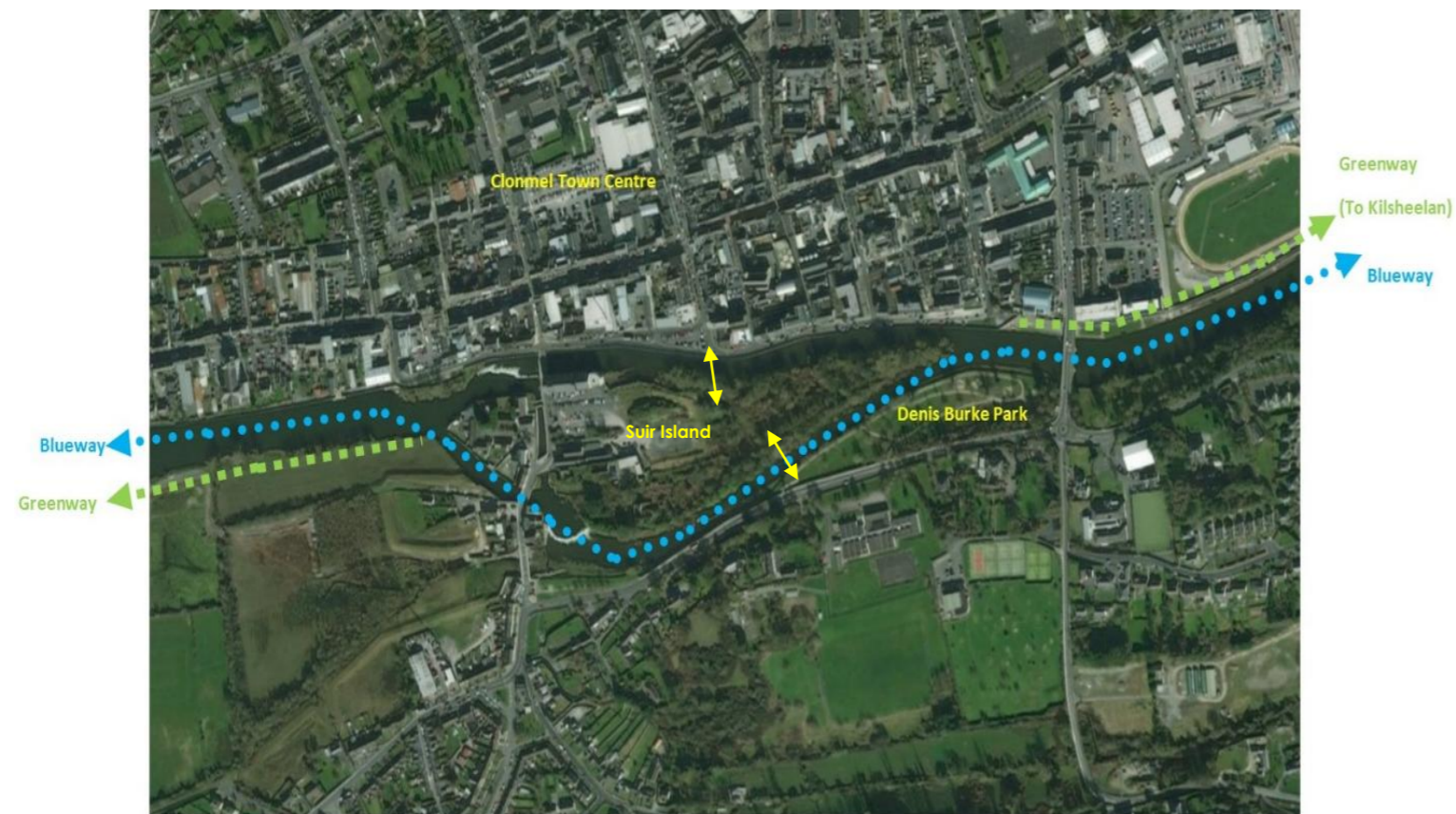


Figure 14: Opportunities for connectivity



Figure 15: Existing trees and river, photo taken September 2017



Figure 16: New river access, view from south bank river/south of Suir Island, November 2018



Figure 17: Slalom Course, September 2017



Figure 18: Existing structures adjacent to Suir Island House (ref: 0803.01-06), September 2017



Figure 19: River wall to south of Suir Island (ref:0904.01-03), September 2017

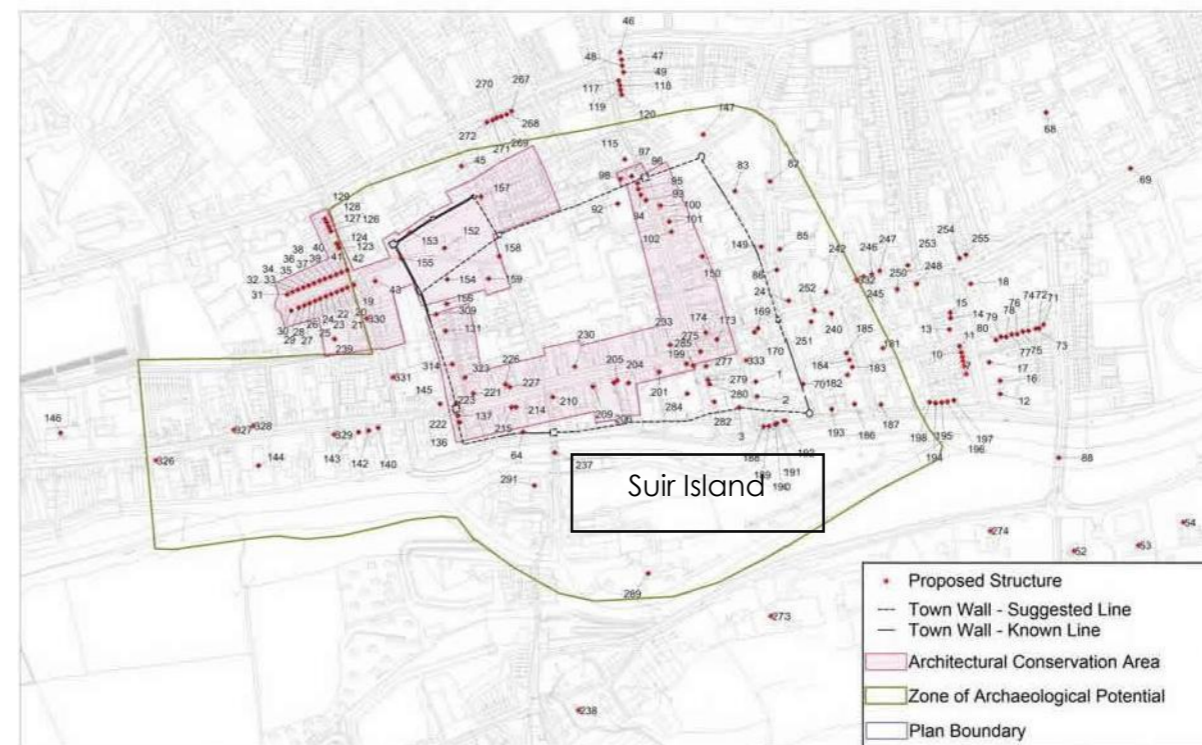


Figure 20: Heritage areas as per Clonmel and Environs Development Plan 2013

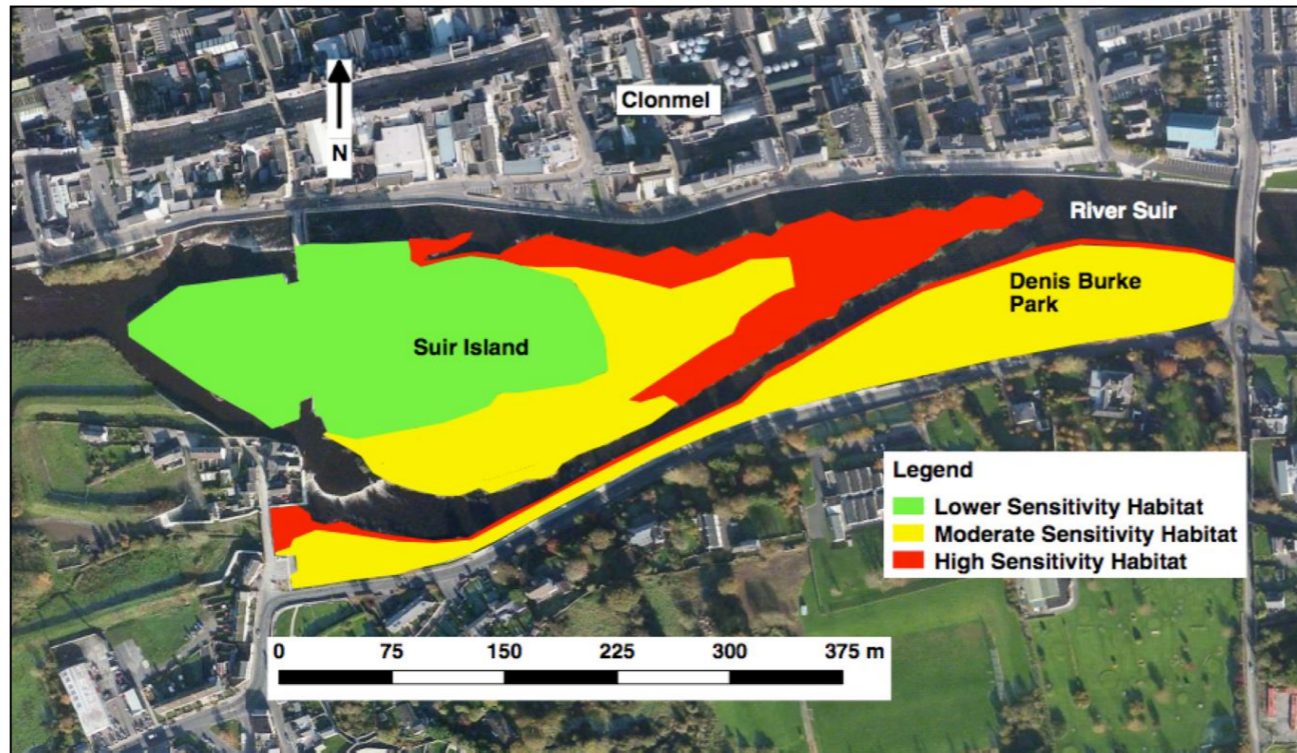


Figure 21: Ecological Sensitivity 'Heat Map'; source Ross Macklin BSc. Env Science, H Dip GIS CIEEM IFM

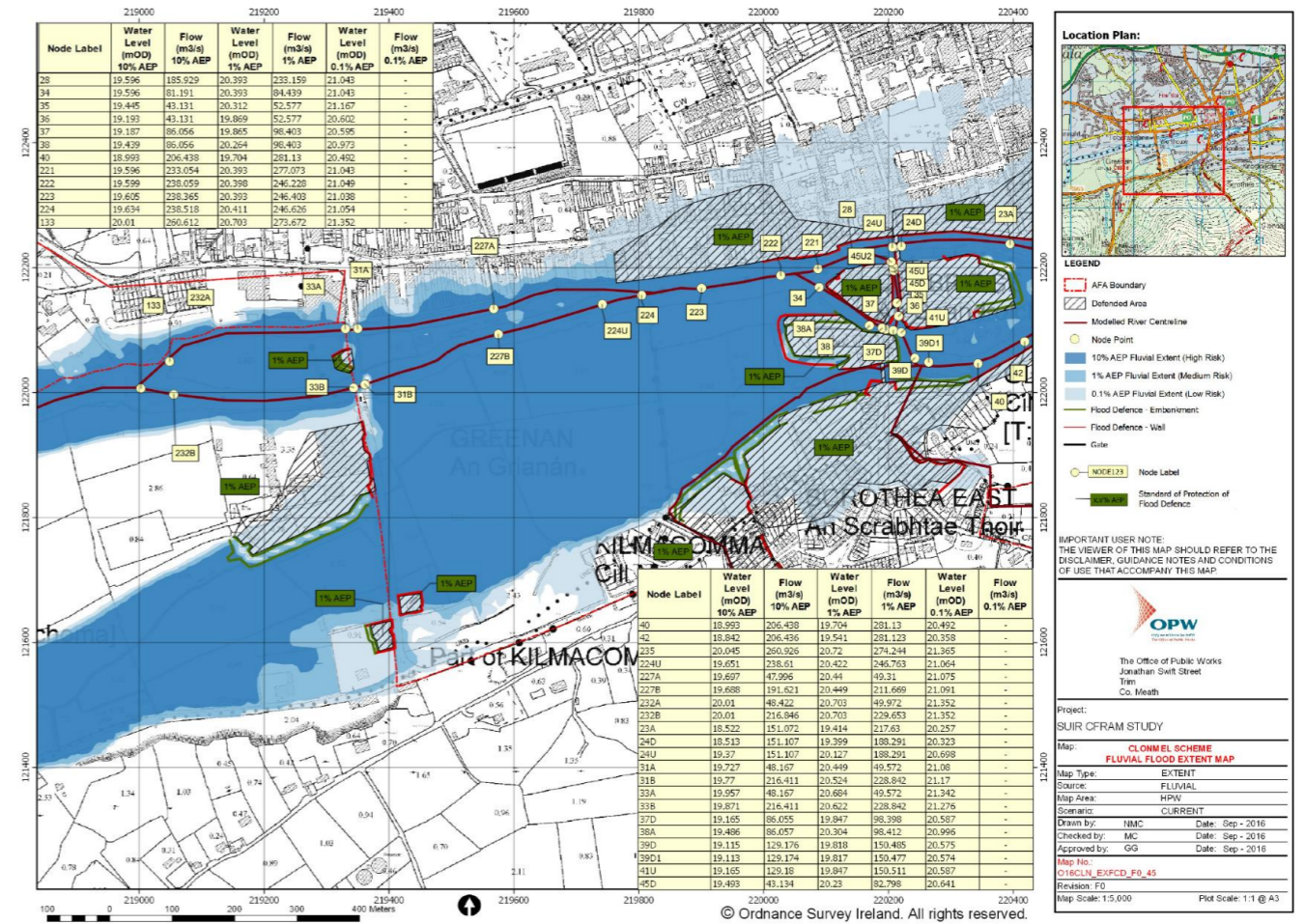


Figure 22: Clonmel Scheme Flood Extent Map, source: OPW

Gathering views and ideas of people's vision for Suir Island

3. Consultation

The development of the Masterplan involved consultation with identified community groups and stakeholders, as well as the general public. The following consultation methods were used:

1. Public workshop and survey day

The design team held a public workshop event as part of a family fun day in Denis Burke Park on Sunday 13th August 2017 (Appendix B). Members of the public were invited to discuss their needs and vision for the island and to complete a survey on same. Following the event, a digital version of the survey (Appendix C) was advertised and remained open for further 3 weeks.

2. Public Survey

A survey was distributed in digital and paper format.

A total of 253 responses to the survey were collected and the main results are summarised in Figures 23 - 25.

3. Stakeholder interviews

Interviews were held with a number of stakeholders to gain their visions, expectations and needs in terms of the use of Suir Island as a recreational and amenity area.

The interviews were conducted in person or by phone and lasted between 20-60 minutes. Notes were taken during the interviews and a summary of the notes sent to each stakeholder after the interview for verification. The relevant stakeholders interviewed were:

- Suircan (Community Arts Network),
- Town Centre Forum,
- Old Bridge Residents' Group,
- Clonmel Canoe Club,
- Clonmel District Salmon & Trout Anglers,
- Inland Fisheries,
- Tipperary Civil Defence,
- TCC Heritage Officer,
- Cahir River Search & Rescue,
- Carrick River Search & Rescue,
- Tipperary Tourism.

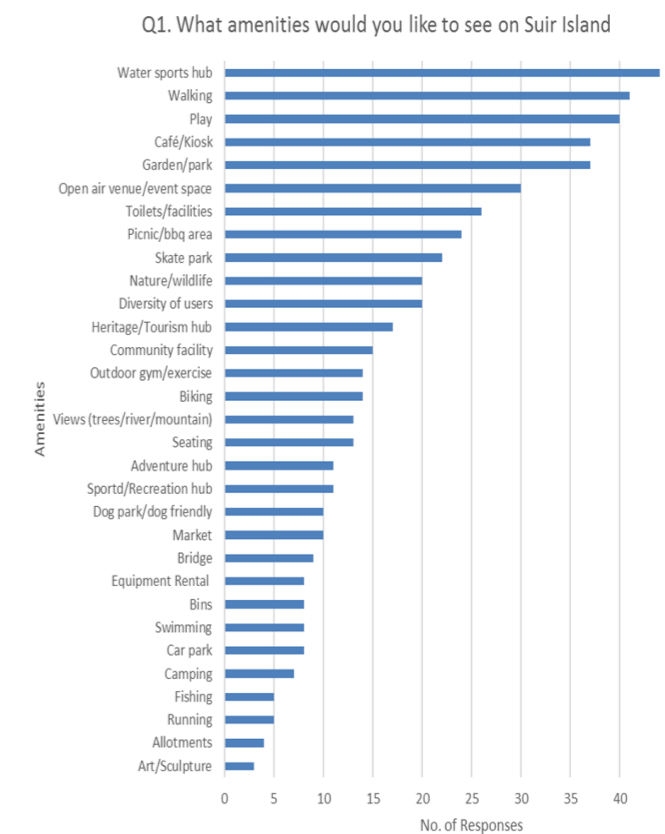


Figure 23: Survey response to what amenities respondents would like to see on Suir Island

Consultation also took place with a number of relevant statutory bodies and other organisations:

- Office of Public Works,
- National Park and Wildlife Service,
- National Monuments Service,
- Local Authority Waters Programme,
- Eir,
- Irish Water,
- Bord Gais,
- Members of Clonmel Borough District.

The findings of the consultation were used to inform the Masterplan proposals and a public meeting was held on Wednesday 25th October 2017 in the Main Guard, Clonmel to present a draft of the proposals to the public and to the stakeholders.

The key findings of the consultation process are summarised below:

- The majority of respondents want to see Suir Island used as a public park for sports and recreation.
- Respondents see the island as a community facility that allows the public to access nature and wildlife and which should be available and accessible to a wide range of user types, groups and ages.
- The role of Suir Island as a source for history, heritage and tourism also featured strongly in the responses, as well as the ability of the island to be a hub for families and children, with children's play areas and water sports also common. The idea of Suir Island acting as the “green heart” of Clonmel was also prevalent, with many respondents viewing the island as a place of peace and relaxation.
- The majority of responses listed recreational and sporting activities such as water sports, walking, a skate park, as well as play areas, café, garden and place for events as preferred activities for the island. Many cited the importance of access to nature and wildlife, and to facilities such as toilets, changing rooms and equipment storage.
- The provision of picnic and seating areas featured as amenities that would appeal to a diverse range of user types and ages.
- Respondents would like to see better access and indicated Sarsfield Street as the preferred location for a bridge crossing.

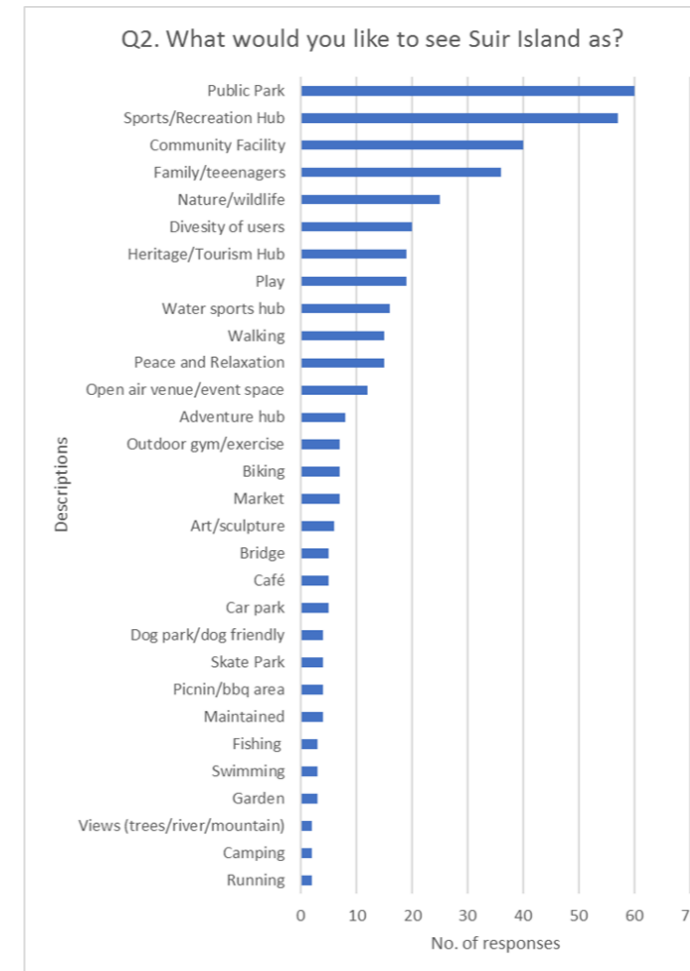


Figure 24: Survey response to what respondents would like to see Suir Island in use as.

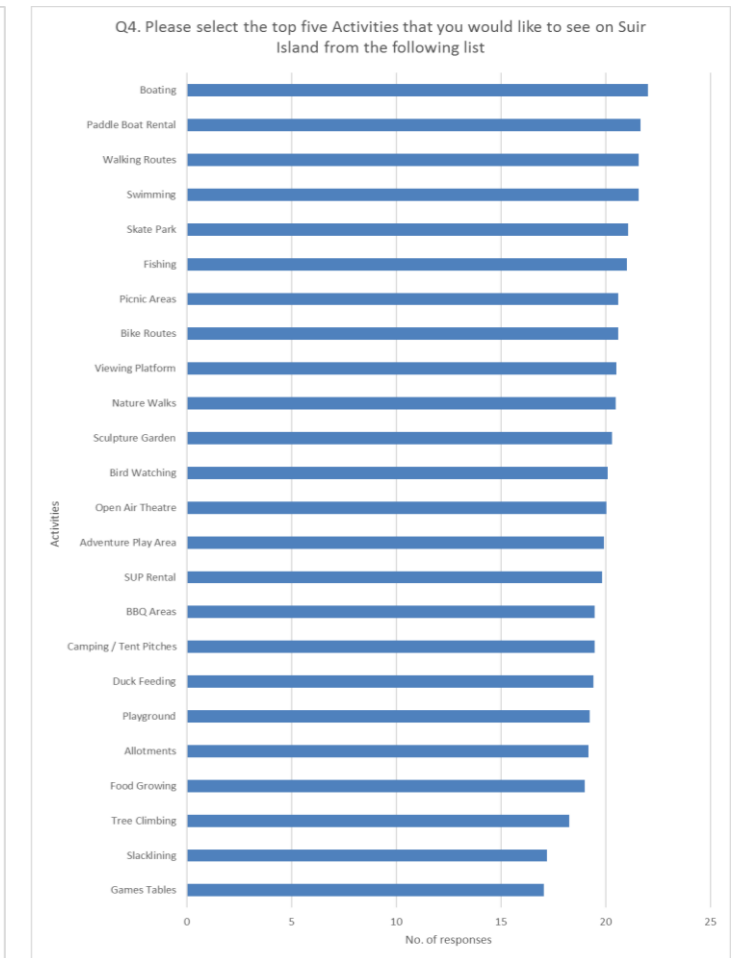


Figure 25: Survey response to respondent's selection of top five activities

4. Proposals

This section details the Masterplan proposals and identifies how the elements could be laid out on the site. This section should be read in conjunction with the scaled Masterplan proposal drawings:

- SK01 Site Layout Plan,
- SK02 Landscape Plan,
- SK03 Site Sections,
- SK04 Facilities Building and Mains Services,
- SK05 Proposal Views,
- SK06 Schematics.

The proposals result from an evidence-based design approach, including the assessment of the opportunities and constraints inherent on the site, the outcomes of the public consultations and the proposed objectives for the site as outlined in the C&EDP 2013 and other documents reviewed. Preliminary habitats and ecological assessments were carried out (See Appendices K and L) which informed the design proposals.

Key Elements Proposed	
Public Plaza and North Bridge	<ul style="list-style-type: none"> • New public plaza to North Quay, linking to Sarsfield Street and Clonmel town centre • Stepped and ramped access to new bridge to north of Suir Island
Raised Walkway around berm and Tree Top Walk	<ul style="list-style-type: none"> • Raised walkway to existing flood berm, with seating • Potential as event space • Starting point for accessible tree top walk into the east of island
Suir Island Garden	<ul style="list-style-type: none"> • Formal garden, seating and picnic provision • Potential uses for Suir Island House
Wildwood clearings and associated pathways/looped walks.	<ul style="list-style-type: none"> • Clearings with range of possible uses (e.g. outdoor classroom to archery, orienteering, bike park, children's play, zip line) • Alternative 'fitness routes' for higher energy walking • Poetry/sculpture trails and seating
Re-water Millrace and develop river crossings and pathways	<ul style="list-style-type: none"> • Re-watering of millrace, with footbridges and stepping stones • Kayak launch/access pontoon and potential loop route for return to slalom starting point • Children's hydrology play area
South Pedestrian Bridge	<ul style="list-style-type: none"> • Pedestrian link bridge (giving clearance for kayakers below) from Suir Island to Southern bank and Denis Burke Park • Seating/slalom viewing possible
Facilities Building	<ul style="list-style-type: none"> • Providing storage, kitchenette, wc's and showers

Table 2: Key Masterplan Proposals

A vision of Suir Island as an amenity and recreational resource

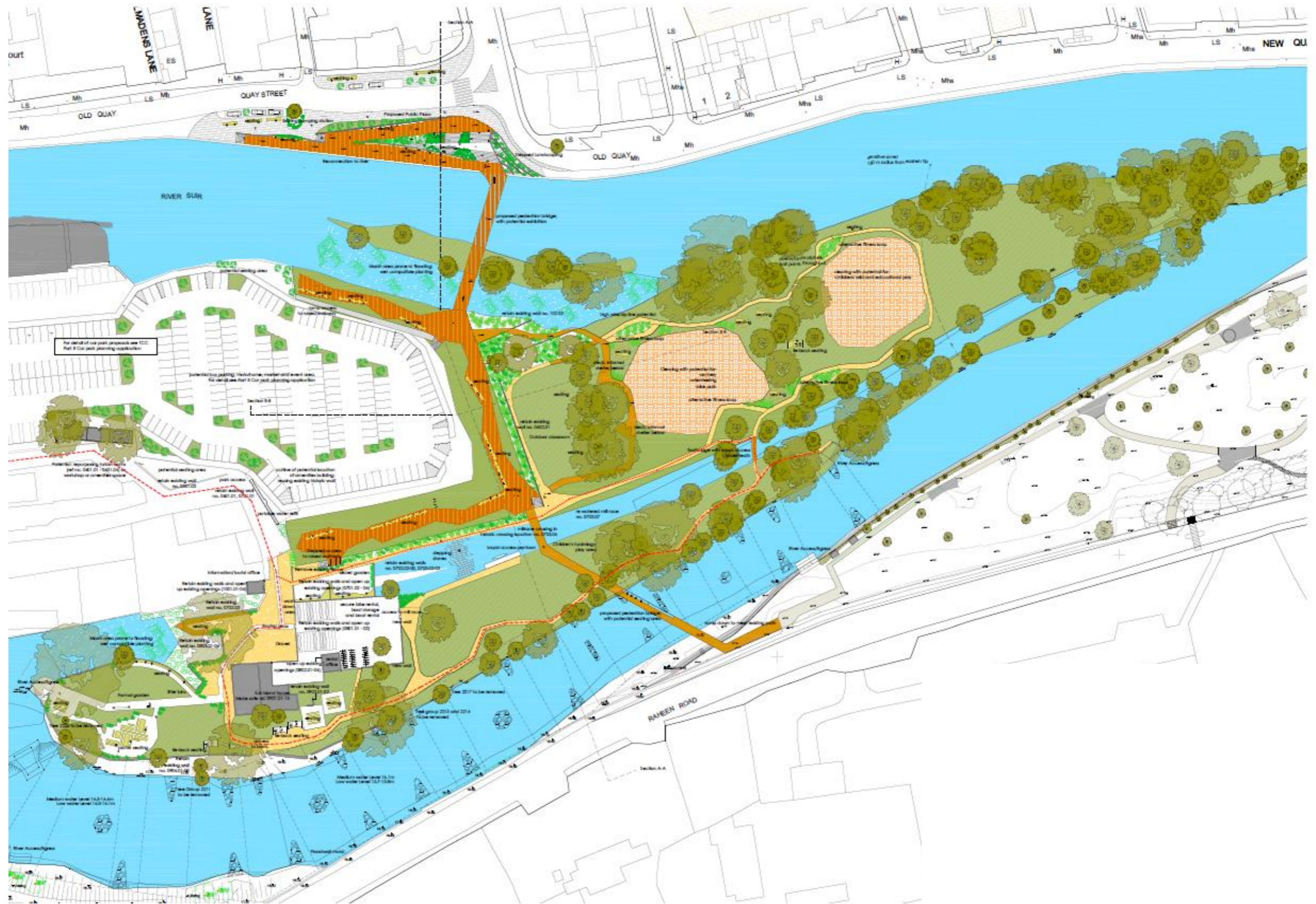


Figure 26: Proposed Site Layout Plan

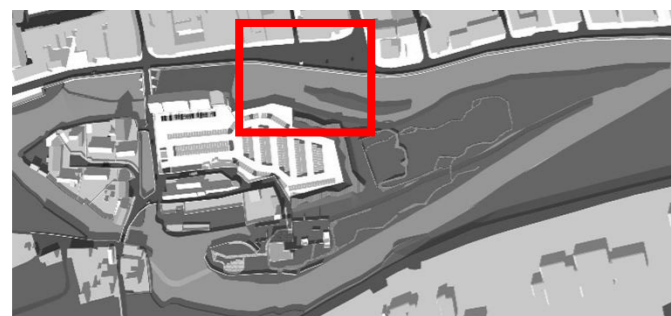


Proposed Site Section A-A (Scale 1:300)



Proposed Site Section B-B (Scale 1:300)

Figure 27: Proposed Site Sections



4.1 Public Plaza and North Bridge

A Public Plaza is proposed with an adjoining pedestrian bridge, a further pedestrian bridge is proposed to link Denis Burke Park south of the island. A 4-metre width is recommended for the bridge to allow for pedestrian and cycle movement and seating. These proposed linkages will improve connectivity to the island, particularly from Clonmel's Town Centre and link to existing amenity lands. The proposed connections also tie in with tourism proposals in the "Flights of Discovery" report - which would see the development of the West Gate, the Museum and the Bulmer's Distillery at Dowd's Lane.

The north bridge has been located so that no trees will require removal to accommodate it.

The proposed design of the plaza involves the removal of a small existing car park to the north of the river and the re-routing of traffic to provide for new public seating and access to the proposed pedestrian bridge linking the quay to the island. The loss of carparking on the Quay will be compensated for with the re-development and extension of Suir Island car park. It is proposed that the new bridge abutment could be designed to permanently provide flood defence for this section of the flood walls, with the flood defence temporary walls slotting in either side when required.

The access ramp is envisaged as a universally-accessible landscaped ramp with seating to provide views of the river. The pedestrian bridge is envisaged as a semi-enclosed space, which is multi-functional space for sitting or for art exhibition.

The design of this plaza involves amendments to the existing one-way system along the Quays near Sarsfield Street. At present the road splits either side of the small car park and flows east on one leg and west on the other, these roads will be merged into a single two-way road north of the proposed plaza. Initial consultation has taken place with the OPW regarding the bridge design. Final bridge design will need to take account of the requirements of the OPW regarding flood defences & flooding, ecological impacts, access requirements, design and materials will need to be considered



Figure 28: Bird's eye view from Sarsfield Street

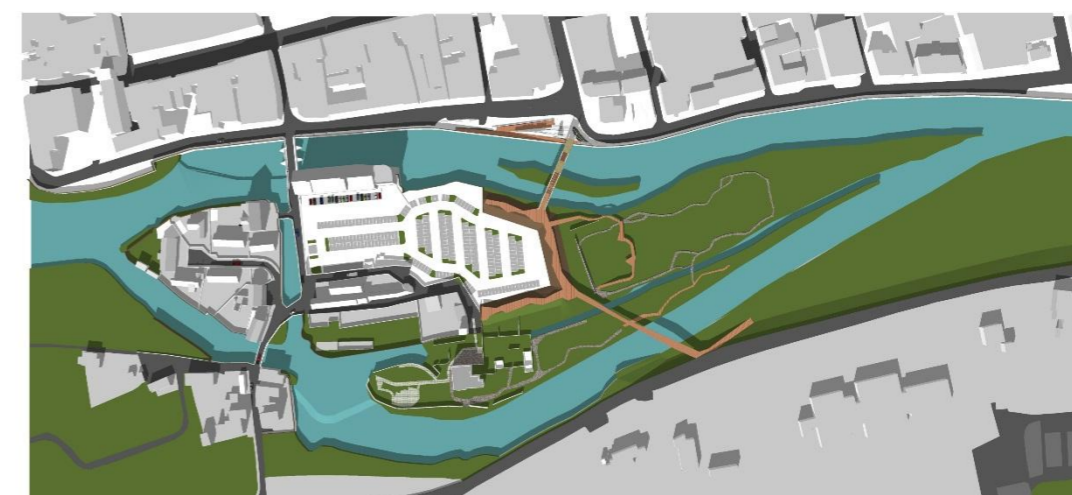


Figure 29: Aerial view of routes

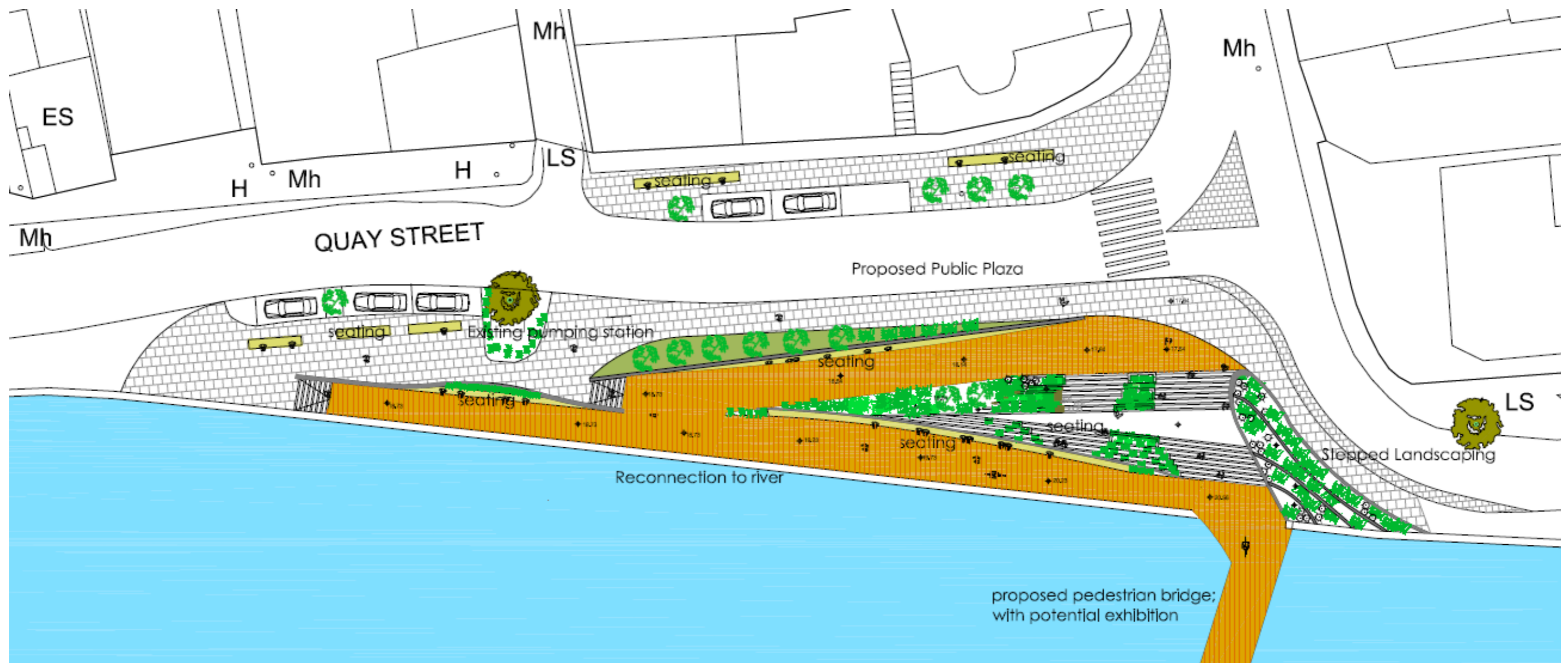


Figure 30: Plaza Site Layout



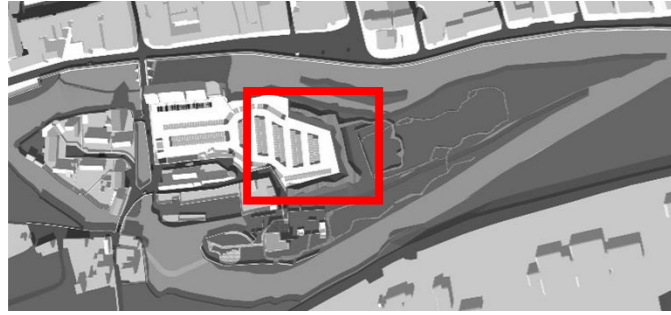
Figure 31: Public plaza/bridge welcoming users onto island (3D render)



Figure 32: Landscaped ramp and steps to bridge and island, with seating and river views (3D render)



Figure 33: Bridge as potential multi-use space; journey to island along semi-enclosed space (3D render)



4.2 Raised Walkway / Event Space

From the north side bridge, a raised walkway with seating and landscaping provides access across the site to the south side bridge. From the raised walk, ramped and stepped access is proposed to both the carpark to the west and the garden and wildwood areas to the south and east. The raised walk envelopes the flood defence berm.

This raised walkway can be used as a departure point for an accessible tree top walk to the east of the flood defence berm, which brings users closer into the tree canopy before returning to ground level in the wildwood area. This area or the adjacent car park area is also seen as a good location for a pop up or mobile coffee shop.

The raised walkway and flood defence berm forms a natural amphitheatre around the car park with potential for same to be used for events i.e. festivals, race start/finish points, community gathering events. Although the adjoining public car park is beyond the scope of the Masterplan it presents opportunities that would complement the Masterplan proposals:

- The car park can accommodate a facilities building providing shower/changing facilities, lockers, welfare facilities and a canteen.
- The car park has potential to cater for bus parking and campervan parking and to be used as space for a food market/craft market and for events, outdoor performances etc.



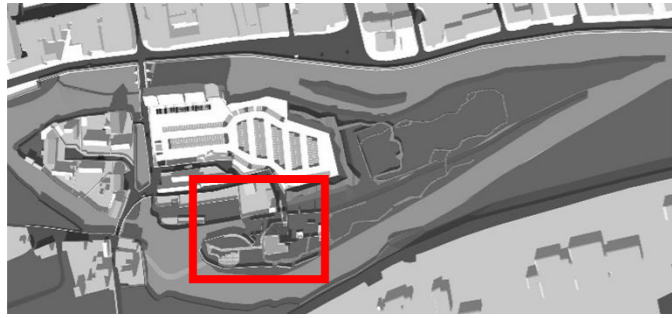
Figure 34: Birdseye view of raised walk and event space from southeast (3d render)



Figure 35: Birdseye view of raised walk and event space from west (3d render)



Figure 36: Raised Walk / Event Space Site Layout



4.3 Suir Island Garden

A formal garden area around Suir Island House, with pathways and managed landscaped areas and a variety of seating types including picnic areas and seating to lie back on. Existing walls in this area are to be repaired and made safe and existing access to the river bank is to be maintained. Site Layout Plan SK01 details the location and number of existing walls/structure and Appendix D details the actions required for the walls/structures as per the 'Blackwood Report'. Structures and walls are to be made safe in the first instance - this is particularly pertinent for Suir Island House.

The current condition of the remaining walls and structures will necessitate a degree of flexibility in determining the most appropriate conservation methodology to be followed to ensure their on-going conservation. Many of the walls and structures are missing sections and others may suffer from structural issues which need to be addressed. The conservation approach must allow for repair, renewal, replacement and partial re-build where appropriate.

In this area several trees of poor condition, or which are weakening the existing heritage walls, are to be considered for removal. Several possible commercial uses and facilities that support the recreational vision for the island have been identified for this area:

- A wash down area for boats and kayaks, as well as provision of potable water for drinking and litter bins.
- An area for boat storing and bike and boat rental which can be made secure and which has good access to the proposed re-watered millrace.
- Existing buildings and the tower structure offer good opportunity for re-use as small rental offices or low-key tourism offices.

The development of secure kayak/boat storage would encourage Blueway users to stop at the island and kayak/boat rental would encourage locals and/or tourists to avail of the recreational opportunities the river offers. This secure storage and rental facility could be easily developed from existing enclosures and structures; as indicated on Site Layout SK01.

It is proposed the re-watered millrace could serve as a safe launching spot for kayak and boat renters or for children's boating as well as providing a shorter route for slalom course users to return to the start of the slalom course.

With the bike rental facilities an opportunity exists to link into the social economy of the area and provide a training opportunity for provision of bike mechanic apprenticeships and the provision of Cytech qualifications in bike mechanics.

Pathways in this area will be constructed from durable high-quality materials and located and designed to avoid damage to tree roots. It is proposed to clear the overgrowth of bramble and sycamore regeneration in this area; see drawing Sk02 Landscape Plan for detail.

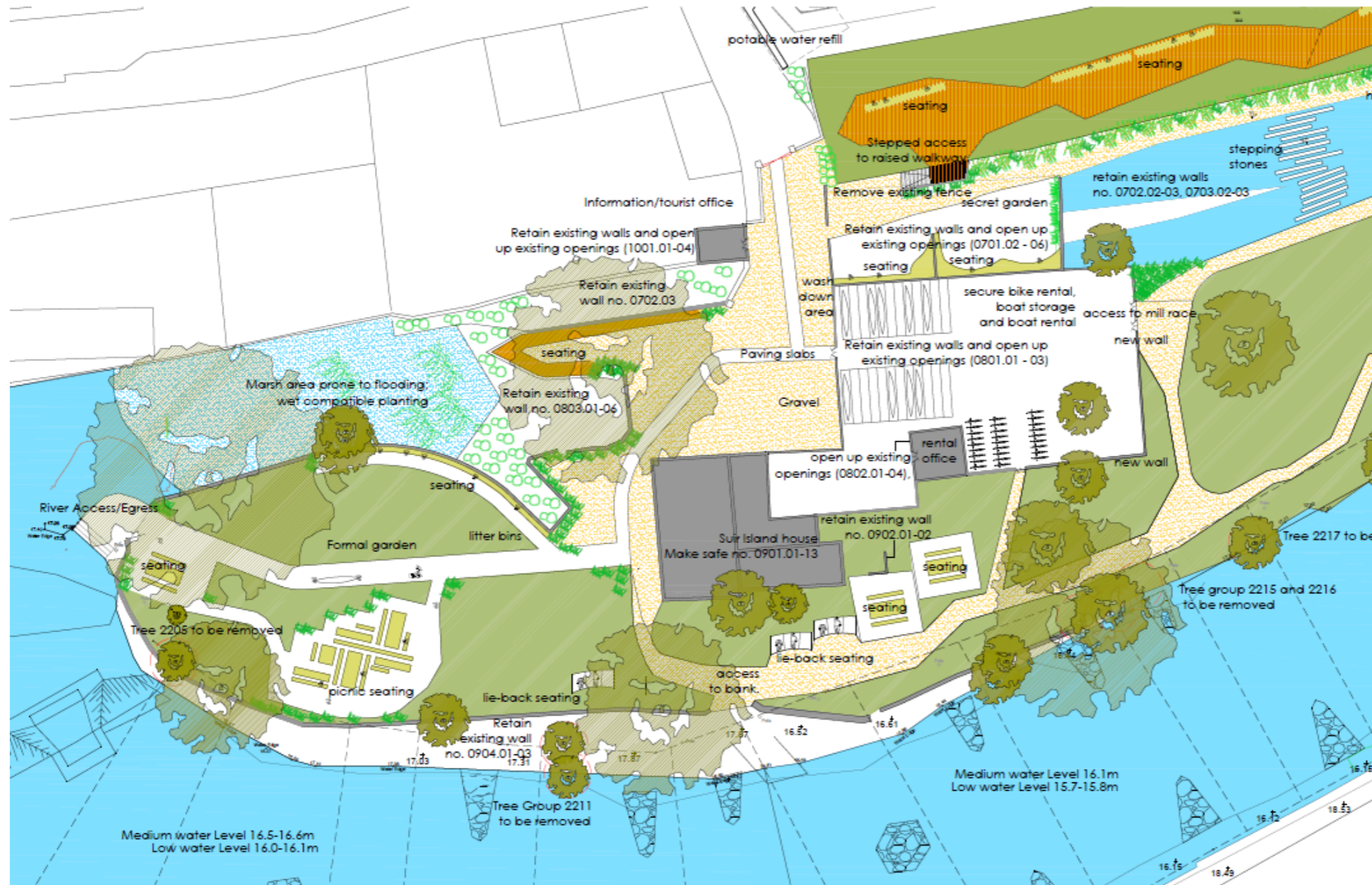


Figure 37: Suir Island Garden Site Layout Plan



Figure 38: Birdseye view of Suir Island Garden Area from east (3d render)

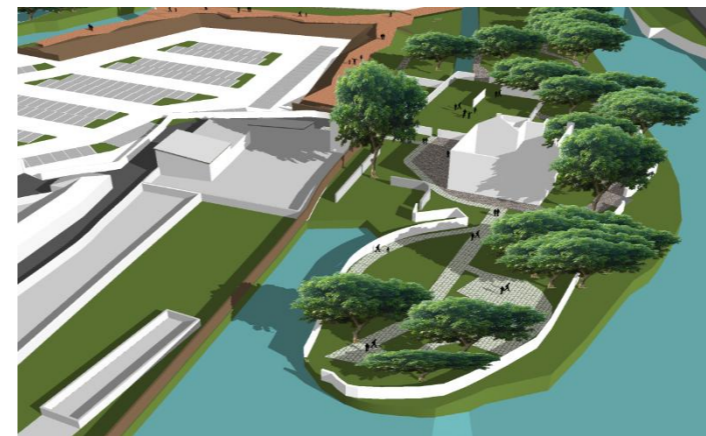
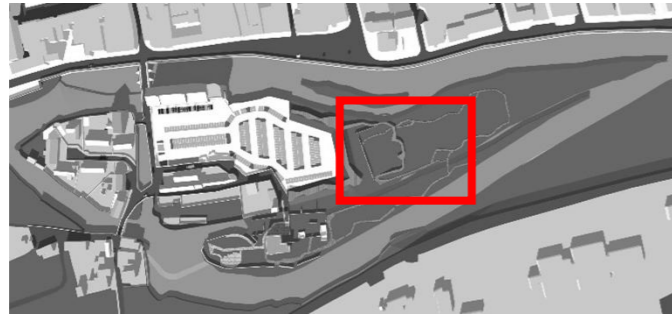


Figure 39: Birdseye view of Suir Island Garden Area from west (3d render)



4.4 Wildwood Clearings, Millrace and South Bridge

Wildwood Clearings

The island is currently quite overgrown but past aerial photos, as well as the tree survey indicate existing clearings. It is proposed that these areas be de-brambled and reinstated. These areas are natural openings in the woodland and as such provide an opportunity for activities.

A range of potential activities are identified below as viable and suitable for the island. These activities are varied in their nature to promote a diversity of use and user:

- Archery,
- Angling,
- Parkour park/skate park,
- Orienteering,
- River adventure course,
- Tree top walk,
- Tree top adventure course/zip line course,
- Natural play areas, with shelter,
- Educational and hydrological play areas,
- Alternative 'fitness loops' off main path with fitness events.

Around the clearings a series of looped paths are identified, which meander through the wood. A variety of path types and directions are proposed to allow a diversity of routes. The location of these paths avoids the root protection areas of the trees, where this is unavoidable a boardwalk can be developed to avoid impact on tree roots. These paths can be used by walkers, bikers or joggers. At intervals, alternative 'Fitness loops' are provided which are integrated to the pathways, which could include more rugged terrain or steps to allow for more energetic exercise.

The series of routes and paths proposed aim to give a variety of routes, movement and access points to the island and to connect not only the island to the town of Clonmel, but to further connect the north of the town to the south.

Natural play areas for children are indicated as well as potential locations for outdoor classrooms.



Figure 40: Bird's eye view of proposed pathways



Figure 41: Wildwood clearings Site Layout Plan

Re-watered millrace

The proposed re-watering of the millrace encourages improved accessibility and user engagement with the river, invokes the history of the island and enhances the recreational and visual amenity of the island.

The re-watered millrace would provide a safe boat launch place for river users and allows the slalom course users to shorten the distance to the start of the course by paddling up the millrace.

Crossing points are proposed across the millrace and the re-watered mill race is in close proximity to the area identified for boat storage and rental. Heritage interpretation can also be development as part of re-watering of the mill to explain the function of mill races.

South Bridge

A second pedestrian bridge is proposed which begins in the location of an historic millrace crossing point. This pedestrian bridge allows access to Denis Bourke Park on the south of the river. This bridge maximises movement and accessibility between Suir Island and the Suir Blueway, Denis Bourke Park and other recreational amenities in the town. The second bridge may involve the removal of two semi-mature sycamores (2225-2226), however given the abundance of sycamore on the island the loss of two semi-mature trees will have no impact on the overall amenity of the island.

Seating is proposed along the bridge, as well as a sufficient width which could potentially serve as informal viewing of the slalom activity. A minimum width of 4m is recommended for both pedestrian bridges to facilitate 2-way walking and cycling, seating etc.

Initial consultation has taken place with the OPW regarding the proposals (Bridges, Berm Boardwalk & Re-Watered Mill Race) and impact of same on the existing flood defences and flood flows in the river. The bridge designs presented will need further consideration and development regarding flood related and other impacts, clearance heights, design and materials etc



Figure 42: View of Clearings amongst trees (3d render)



Figure 43: Re-watered Millrace (3d render)

4.5 Wildwood Area

The eastern part of the island is the most ecologically sensitive area in terms of both flora and fauna. This area is to be left wild with access limited to allow for habitat management only. No public access is proposed for this area.

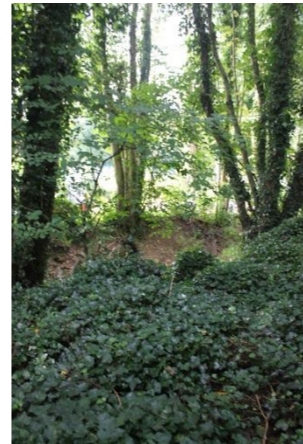
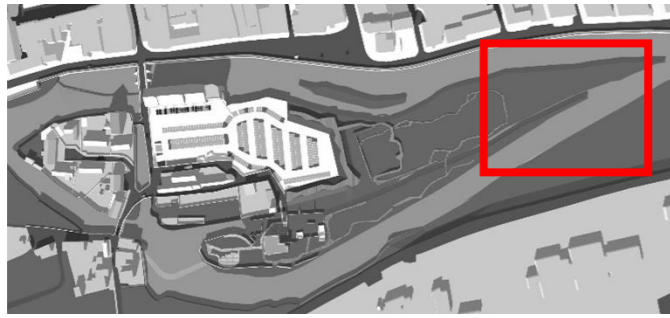


Figure 47: Eastern part of Island as of September 2017

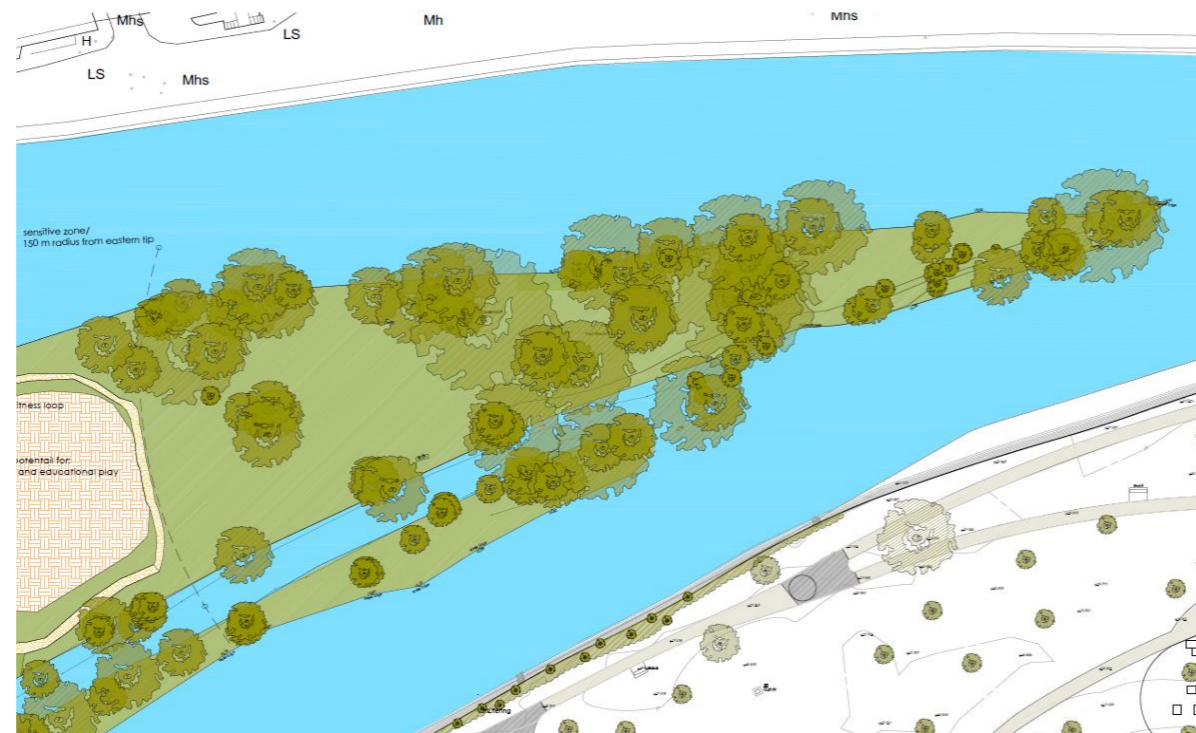


Figure 48: Wildwood Area Site Layout Plan

4.6 Facilities Building

With the intended increased use of the island, additional amenities will be required. A facilities block including storage, kitchenette and male and female wcs and showers is proposed, and its proposed location is outlined in the site layout drawing "SK01 Site Layout Plan".

This location makes use of existing heritage wall no. 0601.01 and 0701.01 (as per 'Blackwood Report') and is in a flood protected area.

It is conveniently located for users of all parts of the island and the car park.

While this building indicates the scale and scope of facilities required, and an indicative location as determined by the site analysis, the final location of the facilities block needs to be determined in consultation with Tipperary County Council and the design will need to be further assessed to determine its archaeological and architectural heritage impact.

It will also be necessary as part of the development of the facilities building and the canoe wash down area in Suir Island Garden area to provide a mains sewer connection for the island, this will be achieved via a dedicated pumping station located adjacent to the facilities building with a pumped rising main connection to the IW infrastructure at the Quays or Raheen road.

IW have responded to a pre-connection inquiry and confirmed that there is capacity within the system for the proposed masterplan. Further consultation with Irish Water is required when detailed designs of the facilities building, and associated services are complete.

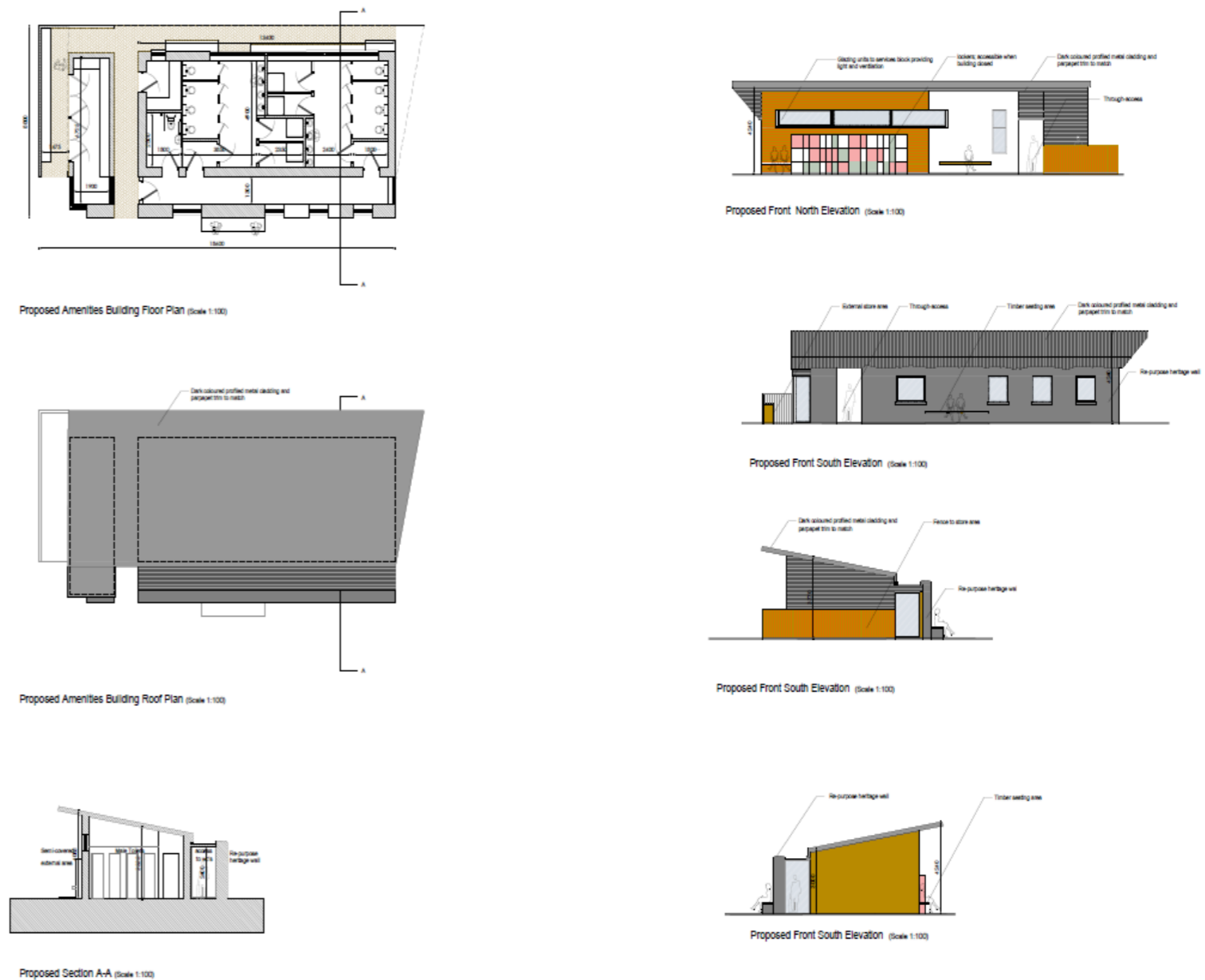


Figure 49: SK04 Facilities Building

4.7 Suir Island House



Figure 50: Suir Island house, photo taken September 2017

The Masterplan proposes that Suir Island House is made safe and that consideration is given to its potential future use. Options in this regard are outlined. These range from the minimum intervention required, to potential future interventions if budget and/or regulation allow.

The development potential of Suir Island House is complicated by both its protected structure status and its location in the flood risk zone of the River Suir.

The house is currently in a bad state of repair, and the primary requirement is to repair it and make it structurally safe for users of the island in advance of any further future interventions.

Minimum Intervention

The minimum intervention is the preservation of the house as a ruin, which would retain its heritage importance. This would involve repairing the walls and making the structure safe for users to enter it at ground level. The addition of internal planting, seating and other landscaping features would integrate the structure into the adjoining formal garden. Information and educational panels could inform users of the heritage and historical value of the house.

This option retains the house as a fully publicly accessible space.

This option would permit flooding of the ground floor and have the minimum cost, complexity and regulatory implications.

Medium Intervention

The medium intervention is the use of the house for several low-key facilities related to storage. This would involve repairing the walls and making the structure safe for users to enter it at ground level. The access points would need to be made secure. The house could be used as clubhouse / store for water sports clubs. The first floor would need to be made good and would need to be accessible as a meeting space. This use would permit flooding of the ground floor, with the first floor above the flood zone. Alternatively, a modern structure could be inserted within the shell.

Such interventions would have medium cost, complexity and regulatory implications.

These uses may have insurance and planning implications. This option makes the house private access only.

Maximum Intervention

The maximum intervention is the re-development and use of the house as a museum, café or other similar use. This retains the house as public access; however, this intervention presents significant cost, complexity and regulatory implications.

This feasibility analysis outlines the potential renovation options for development and use of Suir Island House. The Masterplan proposes the house is secured and made safe for users of the island whilst the other proposals are implemented, and that subsequent analysis and discussion takes place by Tipperary County Council to determine the level of cost and complexity to be invested in the further future development of the house.

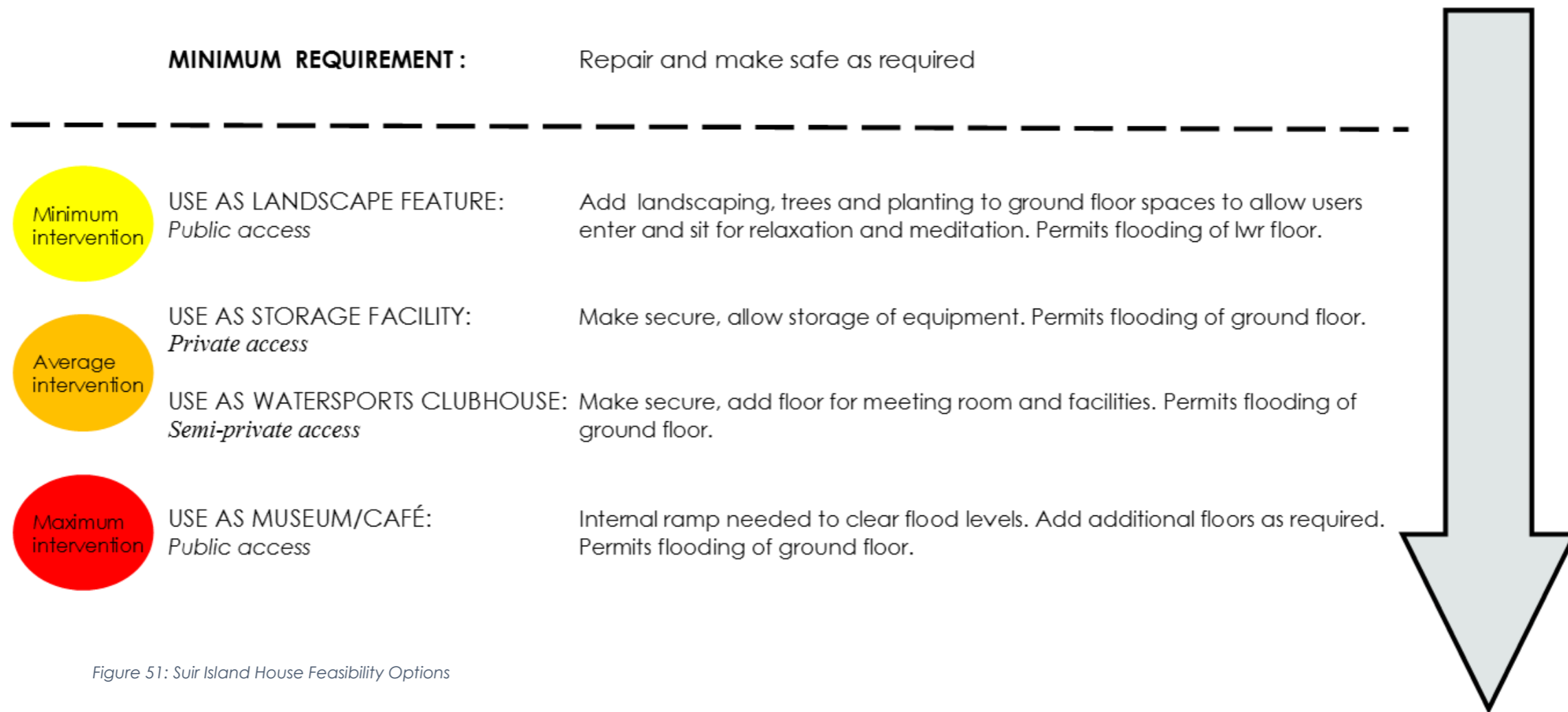


Figure 51: Suir Island House Feasibility Options

4.8 Associated Considerations

With the development of the island the following will need to be considered:

Signage and Lighting

An overall signage plan will be required for the Island to ensure a coordinated approach to signage design is taken. The proposed lighting of the scheme is dependent on the management of the island and accessibility after dark. The Masterplan recommends that lighting is located only along the north-south access bridges, the car park and the boardwalk on the berm. Spot lighting may be located around Suir Island House to emphasise it. Energy saving measures could be incorporated into lighting design. Any proposed lighting scheme will need to consider light impact on existing ecology.

Materials, Paving and Furniture

Choice of materials for the plaza, bridges, raised walkway, paved areas, amenity areas, seating pathways etc will require further consideration in relation to public safety, durability, visual amenity ecological sensitivity, budget and maintenance.



Figure 52: Examples of materials

Landscaping

Landscaping and planting proposals (Landscape Plan SK 02 drawing) has been devised to complement the layout of the site's diverse spaces and ecology. New planting has been designed to connect to existing habitats to provide continuity of habitat and cover. Plant selection has prioritised species that are well adapted to existing conditions, whether native or introduced

Tipperary County Council currently have plans in place to manage Japanese Knotweed, Himalayan Balsam and Giant Hogweed that are in place on Suir Island. In addition, the Masterplan recommends that a plan be implemented to curtail the sycamore regeneration on the island as this would facilitate the regeneration of less vigorous native species. Final design of the island should be based on the principle of reducing surface water run-off and maintaining the natural drainage environment.



Figure 53: Hawthorn Woodland Edge



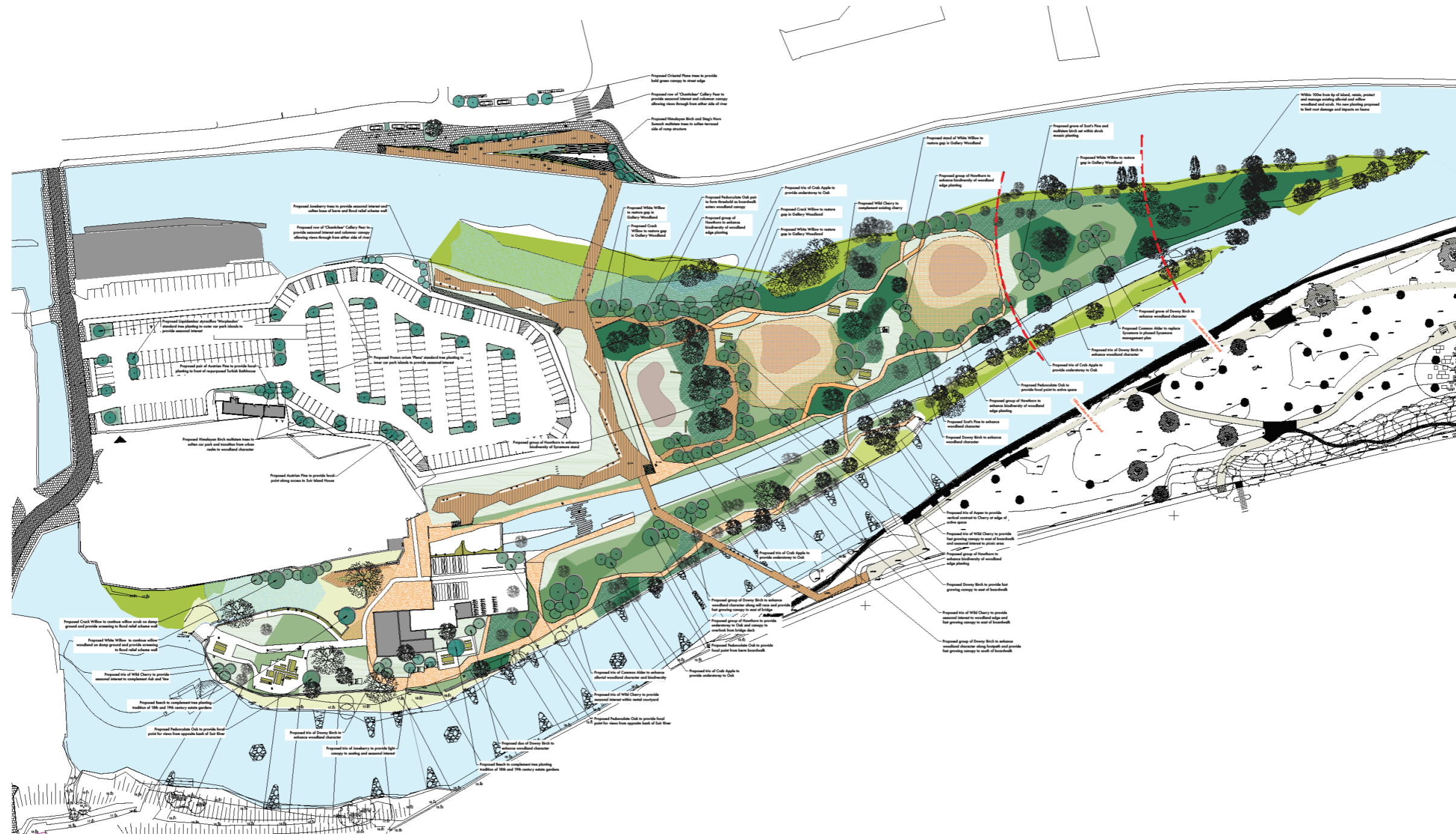
Figure 54 :Downy Birch



Figure 55: Planting examples, LEFT: White Willow, RIGHT: Meadowsweet



Figure 56: Planting examples: TOP: Juneberry in gravel, MIDDLE: Callery Pear trees, BOTTOM: Crab Apple Tree



- LEGEND**
- Existing trees to be retained and protected
 - Existing woodland trees and understorey to be retained, protected and managed to enhance biodiversity
 - Existing woodland understorey to be retained, protected and managed to enhance biodiversity, including the phased removal of species to favour native species
 - Proposed upper canopy woodland tree e.g. Ash, Common Alder, Pedunculate Oak, Sycamore, Willow
 - Proposed medium or medium lower canopy woodland tree e.g. Oak, Apple, Hawthorn, Dogwood, Wild Cherry
 - Proposed shrub woodland planting to include areas and understorey shrub planting e.g. Common Dog, Dogwood, Crataegus, Dog Rose, Holly, Spindle, Wild Rose, Wild Rose
 - Proposed shrub woodland shrub planting to include shrub and field layer planting
 - Proposed shrub woodland field layer planting e.g. grasses, ferns, meadowweet, yellow flag etc.
 - Existing willow woodland and scrub to be retained, protected and managed to enhance biodiversity
 - Proposed willow woodland and scrub to include Alder, Willow, Common Dog, Dog Rose, Dog Willow, Grey Willow, Purple Osier, White Willow
 - Proposed willow scrub to include willow spp. and hydrophilous tall herbs e.g. meadowweet, purple loosestrife, stinging nettle, yellow flag etc.
 - Proposed large standard tree
 - Proposed medium standard / multi-trunk tree
 - Proposed small standard tree
 - Proposed high planting 1.2m to 2.5m height
 - Proposed medium planting 400mm to 1.2m height
 - Proposed low planting 200mm to 400mm height
 - Proposed ground cover planting 100mm to 200mm height

Figure 57: SK02 Landscaping Plan

**Suir Island Masterplan
Landscape Management and Planting Plan**

Traffic Management

The Transport & Access assessments prepared for this Masterplan included a review of the existing road infrastructure including road widths, traffic calming, parking, bus routes and parking as well as pedestrian facilities and links to Clonmel Town Centre. The purpose of the Masterplan development for Suir Island is to incorporate designs for the development and use of Suir Island as a community amenity and recreational resource in Clonmel and to develop the island as a destination within Clonmel. Suir Island is located within the Lower River Suir SAC and is located within the 1 in 100-year flood envelope of the River Suir.

Suir Island is located in the centre of Clonmel and is within easy walking distance of Clonmel Town Centre (O'Connell Street). The island is surrounded by the River Suir on all sides and is only accessible from the town centre via the Old Bridge, a narrow vehicular bridge with limited pedestrian facilities.

Existing development at the island comprises an apartment complex, surface car-park and commercial units. There is a derelict Mill building and an area of private housing on its western portion which is outside the study area.

There is an acknowledged requirement for improved access and connectivity to the island, and for the rehabilitation of existing public facilities on the island. The Clonmel and Environs Development Plan 2013 (C&EDP 2013) sets out a vision for Suir Island that will see it being developed as the recreation and amenity heart of Clonmel, to be ultimately connected to the town centre, Denis Burke Park and the River Suir towpath by providing a pedestrian bridge across the River Suir.

This assessment sets out the strategies for the Suir Island Masterplan which is in line with the Tipperary County Council policies regarding transport & access and pedestrian connectivity for the island and has considered the following documents in preparation of the Masterplan:

- Clonmel and Environs Development Plan 2013,
- Clonmel Traffic Management Plan 2006,
- Clonmel Parking Strategy 2013 – 2020.

Existing Infrastructure

Road Layout

Suir Island is accessed via a single vehicular access road known as Old Bridge Road which links Old Quay / Joyce's Lane as well as O'Connell Street (R884) to the north of the island to the Raheen Road (R680) and Dungarvan Road (R671) to the south of the island. The road is a two-way road facilitating traffic travelling south from the town centre and north into the town centre.

Part of the town centre roads in Clonmel around the island include a one-way system, the existing local road infrastructure around the island is shown in Appendix E.

Road Width

The road widths along Old Bridge Road vary considerably ranging from less than 5.1m at the northern entry point to more than 7m along the central portion of the island. Typical road width is approximately 4.8 to 5.8m. Details of the existing road widths are shown in Appendix E.

Pedestrian Facilities

The existing pedestrian facilities on the island consists of footpath both sides of the road, the footpath widths are limited in areas and vary from 0.85m to approximately 2.7m; this has a negative impact on the accessibility of the island.

Traffic Calming

Operating speeds on the island are controlled by the narrow width of Old Bridge Road and the road alignment which includes a horizontal shift, two vertical ramps and several junctions. Traffic surveys give the mean traffic speed at 34kph, which would suggest the Old Bridge road is 'self-regulating' at present.

Parking

There is an existing public surface level car park on Suir Island which was recently redeveloped and additional parking spaces added. There is also a private car park serving the existing apartments.

The existing and proposed parking arrangements on Suir Island are design to accommodate car only, no provision has been made for bus parking on the island to date.

Bus Routes

There is currently no designated bus parking on Suir Island and no designated bus route to the island. Bus parking in the Clonmel area is provided on Gladstone Street. The island is only accessible to large buses if approached from the south from Raheen Road or alternatively from the north by travelling south along Bridge Street. It is difficult to access the island from the Quays in Clonmel due to turning movement restrictions.

Existing Traffic Conditions

To quantify the existing traffic conditions on the island a series of traffic counts were undertaken on and around the island in September 2017. These counts included junction turning counts carried out on Thursday 7th September 2017. As well as the junction turning counts a series of Automatic Traffic Counts were undertaken between Tuesday 5th September and Monday 11th September 2017. In addition to the above, car park occupancy counts were undertaken on Thursday 7th September starting a 07:00 and at 15-minute intervals thereafter, to identify the total number of vehicles in the car park throughout the day.

Traffic Generation

Because of the potential development of the island as a recreation and amenity resource there will be an increased level of traffic generated to and from the island. It is estimated that the extended car park will generate 413 new daily trips (one way) for weekdays and 343 new trips one way for weekend use.

For the amenity and recreation activities on the island, analysis was conducted on the potential number of people that will attend the site for a selected list of activities based on the proposed Masterplan uses. It has been assumed that the car park can be used as a multi-function space for market stalls, car parking, bus parking and campervan parking and we have also considered visitors associated with the clubs (canoeing & archery), the Blueway and the new amenity park users.

It is estimated that 495 new one-way trips will be attracted to the island for amenity and recreation following implementation of the Masterplan, and the following scenarios have been assessed:

- (a) normal operation of Suir Island Car Park;
- (b) weekday and weekend operation of car park and all recreation and amenity opportunities realised;
- (c) special event hosting.

(a) Normal operation of Suir Island Car Park.

Considering the extended car park only - without the introduction of any of the new amenity or recreation activities on the island - there will be a significant increase in the number of trips to and from the island. Using existing trip rates for the car park extrapolated from traffic count data obtained on the island it can be estimated that some 830 new two-way trips per day will be generated on the island during a typical weekday with some 700 new two-way trips at weekends. Given that the AADT on Old Bridge Road is currently estimated at some 3,300 vehicles per day, this results in an increase in daily traffic of some 20 – 25%. A Traffic Assessment including micro simulation modelling will likely be required prior to the implementation of new traffic management arrangements to facilitate this large increase in traffic, however the design and assessment of these measures is outside the scope of the Masterplan.

(b) Weekday and weekend operation of car park and all recreation and amenity opportunities realised.

With all opportunities realised on the island this scenario could potentially generate weekend trips from the following uses:

- Public Park,
- Suir Blueway,
- Market stalls and customers,
- Tourists and buses or campervans,
- Canoe Club,
- Archery Club.

It is estimated that all the above uses could generate up to 1,000 new two-way trips per day onto the island. This is equivalent to an increase in daily traffic of some 30%. Again, to determine if the road infrastructure on and around the island has capacity to cater for these new trips a full Traffic Assessment including micro simulation modelling is recommended. The implementation of new traffic management arrangements to facilitate this large increase in traffic will likely be required, however the design and assessment of these measures is outside the scope of the current Masterplan.

(c) Special Event Hosting.

The island may host events related to the river and water sports, in particular the slalom course, as well as music or cultural events which may be facilitated in the car park. In these situations, a full event Traffic Management Plan will be required in consultation with TCC and the local Garda Síochána.

Pedestrian and Cyclist Accessibility to the Island & Clonmel Town Centre

The brief for the Masterplan includes for the development of strong connections from the island to the retail and historic core of the Town via safe and direct routes for pedestrians, cyclists and visitors arriving as bus commuters. Safe pedestrian access to and from the island is recognised by the design team as a critical aspect of the development of Suir Island as a community recreation amenity and town centre car park. At present pedestrian facilities are poor and there is no attraction for pedestrian access to the island.

It is proposed as part of the Masterplan to include a new pedestrian bridge linking the Quays and Sarsfield Street to Suir Island and on to Denis Burke Park.

There is also an identified need to provide excellent pedestrian access from the car park to the town centre and O'Connell Street to address the desire lines created by the development of the island. The opportunity to maximise movement and connectivity both within and across the island are detailed in the Masterplan proposals.

Proposed Infrastructure

Because of the implementation of the Masterplan and the creation of an amenity and recreation destination on the island, it will be necessary to undertake a full and detailed Traffic Impact Assessment to consider the need for an upgrade to the existing roads infrastructure around and on the island to accommodate additional volumes of traffic both pedestrian and vehicular that are anticipated.

The Masterplan recommends that a Bus Routing Strategy be considered for the island that would see buses entering the island from the south, off Raheen Road. There is a requirement for works to be carried out to alter the horizontal road alignment on Old Quay, east of the site where a new plaza access for the pedestrian bridge is required.

Routes and Access

Given that it is proposed as part of the Masterplan to include bus parking on the island, it is necessary to consider the routing of buses to the island car park and this must be done in the context of the existing and proposed traffic management measures including the physical restrictions on the local network in particular the turning movement restriction at the junction of Old Bridge Road and the Quays.

In light of these considerations it is proposed that Buses coming to Clonmel for tourist day trips and the like which are coming from the west and the N24 from Limerick or Tipperary Town are routed over Workhouse Bridge then west along the Dungarvan Road and onto the island.

Buses travelling to Clonmel from the east and the N24 from Waterford & Kilkenny will be routed over Gashouse Bridge and west along Raheen Road to the island.

Potential for both vehicular and pedestrian emergency access/maintenance access is proposed as indicated in Figure 59. This allows emergency access to the active parts of the island

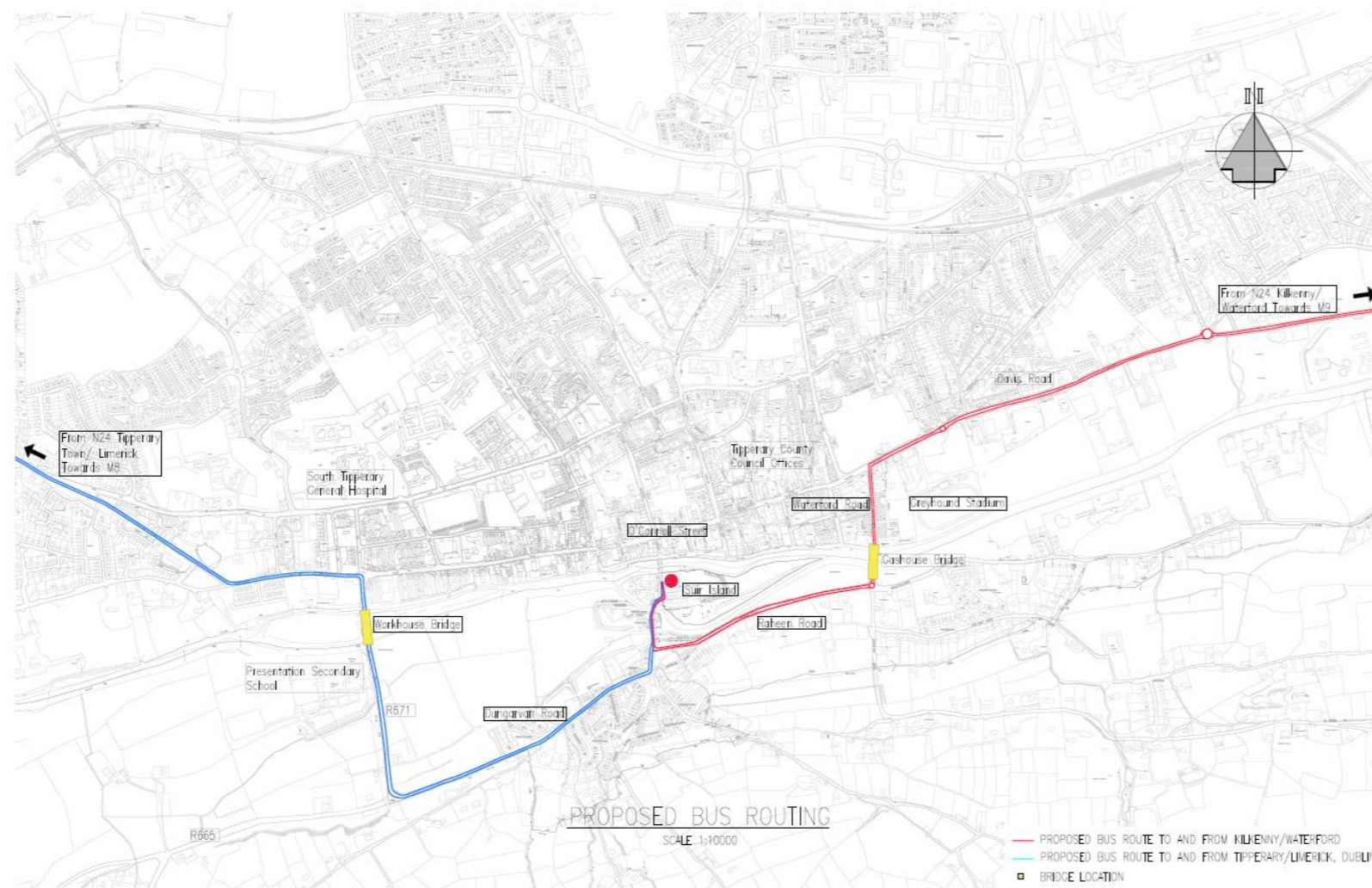


Figure 58: Proposed Bus Routing



Figure 59: Potential Vehicular and Pedestrian emergency access routes

5. Implementation and Phasing

This section sets out the phasing, implementation and funding strategy for the development site.

The masterplan does not seek to set out the detailed design of the site at this stage, rather it identifies the key design elements (such as the bridges, walkways, gardens, access routes etc.) and illustrates how the development of the island can be laid out. The Masterplan also identifies infrastructural and facility requirements that will need to be addressed to ensure that lands can be developed to their full potential.

The proposals in the Masterplan need to be developed further to detailed design and following this planning consent to undertake development must be secured. Any consent application will need to deal with applicable statutory requirements.

5.1 Cost

A cost estimation has been prepared which estimates a cost for the whole scheme at €9,580,000 (including a contingency sum of €900,000 and excluding VAT). A breakdown of these costs is given in Table 3 below, and more detail can be found in Appendix F. These figures are based on the following assumptions, qualifications and exclusions.

Assumptions

- competitively priced tenders will be obtained for the project,
- ground conditions are normal, and no specialist foundations are required for the new Facilities building.

Considerations in the implementation of the Masterplan

Qualifications

- The estimated construction costs include an allowance for Mechanical and Electrical Installations, however, the exact scope of work will need to be determined and the figures confirmed by a Services Specialist,
- The estimated costs were based on wage rates and material prices at 2018 prices and exclude inflation from then,
- The estimated costs include for works to the new and existing buildings as per the Site Layout Plan (Ref 1738 SK01) which take account of the existing Suir Island House, the new Facilities Buildings and tourist and rental offices,
- Stage 7, Tree Top Walk includes for an elevated walkway at canopy level 200 metres in length with a provision for a zipline, as per the Site Layout Plan (Ref 1738 SK01). These costs will need to be reviewed on receipt of a detailed design,
- Costs included for the timber boardwalk within the wildwood areas, Stage 3, allow for the detail as per Section A-A on Site Layout Plan (Ref 1738 SK01). The Boardwalk within Stage 4 is considered to be an elevated platform and increased provisions for same are included here,
- Works associated with the carpark including services do *not* form part of these costs and are to be carried out directly by Tipperary County Council,
- The project is currently at Masterplan stage and developed designs will need to be monitored and our estimates reviewed when more information is available and the structural and services solutions are resolved.

Exclusions

The following items are specifically excluded from the cost estimate:

- inflation from 2018,
- local authority fees and charges,
- contributions to Public Utilities (if any),
- professional fees and expenses,
- finance costs (if any),
- legal fees (if any),
- advertising costs (if any),
- car parking facilities,
- removal / diversion of any existing services,
- removal of any contaminated materials discovered during the works,
- archaeological investigation and associated attendance costs,
- Ecological Impact Assessment, if required.

5.2 Implementation Stages

The Masterplan is designed to allow for various elements to be developed independently of each other. Certain elements are inter-dependent i.e. access to the wildwood clearings area is dependent on the development of the raised walkway or development of the river crossing from Suir Island Garden area to the south.

The Table below sets out the main elements proposed, key actions and deliverables and identifies the costs associated with same.

ELEMENT	ACTIONS/DELIVERABLES	COST
Public Plaza and North Bridge	Structural design, plan, statutory compliance, implement.	€2,850,000
Raised Walkway around berm and Tree Top Walk	<ul style="list-style-type: none"> • Addition of raised walkway, planting and furniture; • Addition of access ramps and steps. • Design, plan, statutory compliance, implement. 	€2135,000
Suir Island Garden	<ul style="list-style-type: none"> • Make Suir Island House and associated walls and structures safe, including removal of overgrowth; • Costs included would allow for average intervention as detailed in Section 4.7, to include for storage and meeting spaces with private access only. • Clear overgrowth where necessary, including invasive species management; • Addition of paths, planting and furniture. 	€1,360,000 (of which Structures/ Services: €667,000)
Wildwood clearings and associated pathways/looped walks.	<ul style="list-style-type: none"> • Advertise and consider applications for activities • Evaluate proposed activities in relation to overall masterplan; • Implement activities. 	€260,000
Re-water Millrace and develop river crossings and pathways	Design, plan, statutory compliance, implement	€225,000
South Pedestrian Bridge	Design, plan, statutory compliance, implement	€1,650,000
Facilities Building	Design, plan, statutory compliance, implement.	€200,000

Table 3: Implementation Stages

5.3 Next Steps

Action	Timeline Target
<p>Establish a Project Steering Group within Council</p> <p>Ensure a multidisciplinary Project Team is established and resourced to allocate time to the project.</p> <p>Appoint a Project Lead</p> <p>Provide and obtain 'enabling funds' that can be allocated to progressing the project stages</p> <p>Identify stakeholders and partner organisations to work in delivering the Masterplan.</p>	End of 2019
<p>Reinforce the objectives of the Masterplan through amendments and revisions to planning policy documents.</p> <p>To direct implementation of the Masterplan through appropriate development management and Development Plan policies.</p>	Ongoing
<p>Prioritise project elements for investment and delivery in the short, medium to long term.</p>	End 2019
<p>Progress the overall development design or design of key elements to a stage that is sufficient to allow for application for planning consent.</p>	Mid 2020
<p>Secure planning consent for the overall development or key elements of the development.</p> <p>Note: This likely to require application to An Bord Pleanála pursuant to Section 177AE of the Planning and Development Act 2000, as amended.</p>	End 2020-1 st Quarter 2021
<p>Capital Funding streams to be sought to progress the delivery of project elements</p> <p>Potential Funding Sources: URDF, Historic Towns Initiative, Leader Rural Development, Structures at Risk Fund etc.</p> <p>Note: Match Funding will be required</p>	Ongoing
<p>An Annual Maintenance Programme is to be developed to guide the maintenance of the existing buildings and habitats on site to an appropriate and practicable standard.</p>	Ongoing

Table 4: Next Steps

Notes

